

COMPLETE STREETS POLICY AND IMPLEMENTATION PLAN

The Borough of Pompton Lakes
January 2019



Michael Baker
INTERNATIONAL

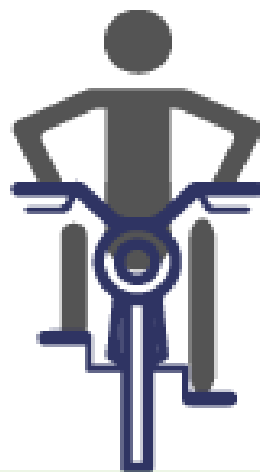
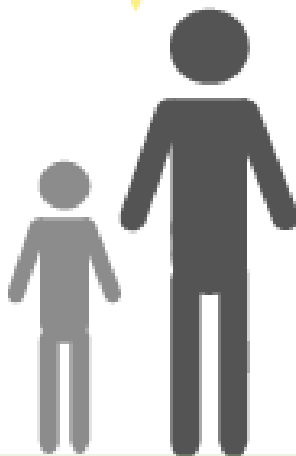
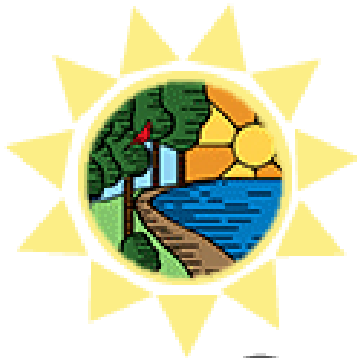


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EXECUTIVE SUMMARY

The Borough of Pompton Lakes (Pompton Lakes) is working to adopt a Complete Streets Policy and develop a Complete Streets Implementation Plan. Adoption of a Complete Streets policy formalizes the Borough's desire to plan, design, implement and maintain roadways that are safe for users of all ages and abilities. A Complete Streets Implementation Plan provides the framework to assess the street network currently in Pompton Lakes, assign responsible party, and set priorities and action items. Together, the policy and plan are a tool for the community to discuss the possibilities with community leaders, residents, and other agencies including the Passaic County and the state. Additionally, it assists in the application and selection process for many funding opportunities and as grant opportunities become available, the Implementation Plan provides a list of ready concepts which can be identified based on the type of funding.



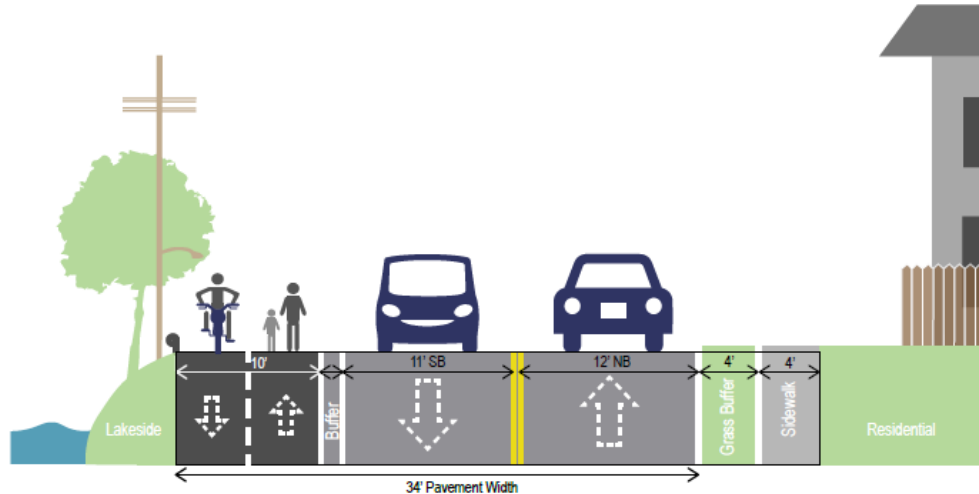
The Pompton Lakes Complete Streets Implementation Plan identifies potential opportunities and deficiencies along roadways in Pompton Lakes that could be improved to encourage safe walking and biking, recommends potential improvements, and provides an action plan to implement these improvements. Several roadway and intersection treatments such as roundabouts, raised intersections, and bike lanes have been recommended in this plan. These treatments are context-sensitive design recommendations that were selected to fit the unique needs along the priority corridors and at specific intersections based on data collection and findings. This range of treatments were recommended to improve safety for all roadway users, calm traffic, improve pedestrian and bicycle safety access and mobility, and create a more livable, active community in Pompton Lakes.

A public survey that was distributed to Pompton Lakes residents showed that over 74% of those surveyed are concerned about missing and insufficient sidewalk, 66% are concerned about speeding and aggressive driving. 55% are concerned about the insufficient space for bicyclists and over 30% are concerned about missing or insufficient crosswalks, and insufficient lighting. The concerns of residents can be addressed through the implementation of a policy and this corresponding plan.

Based on survey results received from the project steering committee, priority corridors were identified for this study. These corridors are either near Pompton Lakes Schools, the Central Business District, Parks and Recreation spaces or need infrastructure improvements. The nine corridors identified are:

- ▶ Dawes Highway
- ▶ Paterson-Hamburg Turnpike
- ▶ Lincoln Avenue
- ▶ Ramapo Avenue
- ▶ Riverdale Boulevard
- ▶ Wanaque Avenue
- ▶ Colfax Avenue
- ▶ Lakeside Avenue
- ▶ Ringwood Avenue

Through the data collection and research conducted in Pompton Lakes, the following are the main recommendations and improvements that should be prioritized to create a safe, happy and healthy community. First and foremost is the adoption of the Complete Streets Policy. This will assist in obtaining funding for infrastructure improvements and programming for the recommendations that follow.



- ▶ **Adopt a Complete Streets Policy and Implement the Pompton Lakes Complete Streets Plan.**
- ▶ **Install sidewalks where missing.**
- ▶ **Provide bicycle facilities within the roadway network using bicycles lanes, “sharrows,” buffered bike lanes, and shared-use paths where appropriate.**
- ▶ **Implement traffic calming measures at recommended intersections.**
- ▶ **Install raised intersections and raised crosswalks at recommended intersections.**
- ▶ **Upgrade intersections to include ADA compliant curb ramps, pedestrian signal heads and push buttons, pedestrian scale lighting, and high visibility crosswalks.**
- ▶ **Implement roundabouts at recommended intersections.**
- ▶ **Develop ongoing educational, encouragement, and enforcement programs to increase motorist, pedestrian, and bicyclist awareness of the rules of the road.**

1. INTRODUCTION

The Borough of Pompton Lakes (Pompton Lakes), Passaic County, received a grant from the New Jersey Department of Transportation’s (NJDOT) Office of Bicycle and Pedestrian Programs (OBPP) to develop a Complete Streets Policy and Implementation Plan. The primary goal is to develop a Borough-wide Complete Streets Implementation Plan. The adoption of a Complete Streets Policy is a primary action item of the plan.

Michael Baker International Inc. (Michael Baker) was requested to provide Local Technical Assistance to Pompton Lakes. The Local Technical Assistance Program is governed by the goals and objectives of the New Jersey Statewide Bicycle and Pedestrian Master Plan and the NJDOT Complete Streets Policy.

The Bicycle and Pedestrian Master Plan has the following goals:

- **Improve Safety:** “Eliminate pedestrian and bicycle fatalities and serious injuries and improve the sense of safety experienced by all who bike or walk.”
- **Enhance Accessibility, Mobility & Connectivity:** “Provide a connected and accessible network for bicyclists and pedestrians throughout New Jersey.”
- **Achieve Healthy Sustainable Communities:** “Provide opportunities for people to become more healthy and active through walking and biking.”
- **Foster a Culture Shift:** “Considering the needs of all users becomes the default way of doing business, with Complete Streets integrated into everyday practice.”
- **Facilitate Coordination & Integration:** “No one entity alone can achieve the goals of this master plan – a partnership of public, private, and nonprofit partners is needed.”

The goals are factored into the bicycle and pedestrian planning and concept development process as appropriate.

The Statewide Bicycle and Pedestrian Master Plan is available online at:

<https://www.state.nj.us/transportation/commuter/bike/pdf/bikepedmasterplan2016.pdf>

The NJDOT Complete Streets (CS) Policy was finalized in December 2009 and requires that future roadway improvement projects include safe accommodations for all users, including bicyclists, pedestrians, transit riders, and the mobility-impaired. According to the NJDOT Complete Streets Policy, Complete Streets can be defined as:

“Means to provide safe access for all users by designing and operating a comprehensive, integrated, connected multi-modal network of transportation options.”

Complete Streets can be beneficial to individual’s health, safety, equity, economic vitality, transportation choices, environment, mobility and livability. The NJDOT CS policy is implemented through the planning, design, construction, maintenance, and operation of new or rehabilitated transportation facilities within public rights-of-way that are federally, or state funded, including projects undertaken or administered by NJDOT. Goals and Objective for this project are as follows:

Goals:

- Develop and adopt a Complete Streets Policy.
- Develop a Complete Streets Implementation Plan.
- Identify programs and improvements to promote safe biking and walking.

Objectives:

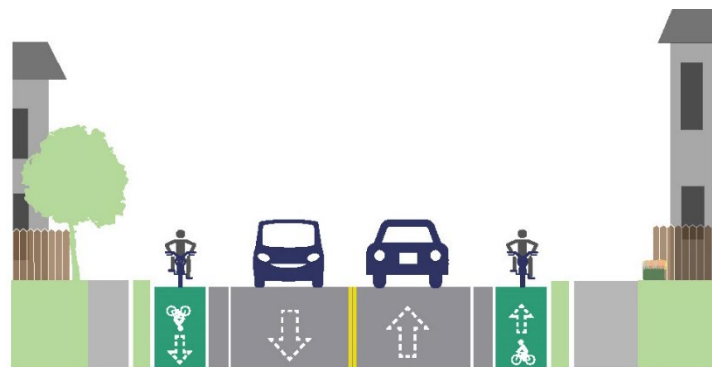
- Identify potential opportunities and deficiencies along roadways within Pompton Lakes that could be improved to encourage safe walking and biking.
- Recommend potential infrastructure improvements.
- Prioritize and develop an action plan to implement the recommended improvements.

1.1 WHAT ARE COMPLETE STREETS?

The Passaic County Complete Streets Design Guidelines defines Complete Streets as streets for everyone, designed to provide safe access for all users including motor vehicles, pedestrians, bicycles, and transit riders. This also includes mobility for persons of all ages and abilities. Complete Streets make it easy to cross the street, walk to shops, bicycle to work, and operate motor vehicles in a safe way.

BENEFITS OF COMPLETE STREETS

- Increased Safety
- Equity for non-drivers
- Health Benefits
- Increased quality of life
- Mobility for all users
- Economic vitality
- Environmental Benefits



PURPOSE OF THE POLICY AND PLAN

A Complete Streets policy formalizes a community’s desire to plan, design, implement and maintain roadways that are safe for users of all ages and abilities. There are several required components to a Complete Street Policy. These include... a vision of how the community wants roadways to look and how they plan to implement those features, defines “all users” to include pedestrians, bicyclists, transit riders of all ages and abilities, encourages street connectivity, projects of application should include new and retrofit projects, adoptable by all relevant government agencies, sets a clear procedure to address exceptions, and utilizes the latest and best design standards. A Complete Streets policy states that current and future projects will be planned, designed and implemented for all users of a roadway.

A Complete Streets implementation plan provides the framework to assess the street network currently in Pompton Lakes, assign responsible party, and set priorities and action items. Together, the policy and plan, are a tool for the community to discuss the possibilities with community leaders, residents, and other agencies including the county and the state. Additionally, it assists in the application and selection process of many funding opportunities. These funding opportunities can assist with infrastructure improvements along with educational and enforcement programs for the community. As grant opportunities become available, the Implementation plan provides a list of ready concepts which can be identified based on the type of funding. For instance, concepts which address safety concerns can be funded through the Highway Safety Improvement Program. Similarly, improvements near schools can qualify for the Safe Routes to School (SRTS) Grants program. As part of the scoring criteria, the SRTS Grant program awards extra points for municipalities which have a Complete Streets Policy and extra credit for those municipalities which demonstrate past implementation of Complete Streets initiatives.

The Pompton Lakes Complete Streets Implementation Plan includes an inventory of existing conditions, results of a public outreach effort, identification of priority corridors and improvement locations, and ultimately a priority list of infrastructure improvements.

1.2 SCOPE OF WORK

The purpose of the Complete Streets Policy and Implementation Plan is to identify potential opportunities and deficiencies along roadways within Pompton Lakes that could be improved to encourage safe walking and biking, recommend potential improvements, and develop an action plan to implement recommended improvements. As part of this effort, Michael Baker performed the following tasks:

- **Public Outreach**
 - Project Steering Committee
 - Internet-Based Public Involvement
 - Public Information Center
- **Data Collection**
 - Obtain Existing Resources
 - Complete a Field Inventory and Investigation
- **Existing Conditions Analysis**
 - Sidewalk Inventory and Assessment
 - Bicycle Compatibility Assessment
 - Intersection Assessment
 - Bicycle and Pedestrian Crash Review
 - Local and Regional Connections
- **Develop Recommendations**
- **Complete Streets Policy and Implementation Plan**

2. DATA COLLECTION

2.1 PRIORITY CORRIDORS

Based on survey results and the public input received during National Night Out 2017, as well as input from the project steering committee, priority corridors were identified for this study. These corridors are either near Pompton Lakes Schools, the Central Business District, Parks and Recreation spaces or are in need of infrastructure improvements. The nine corridors identified are:

County Roads

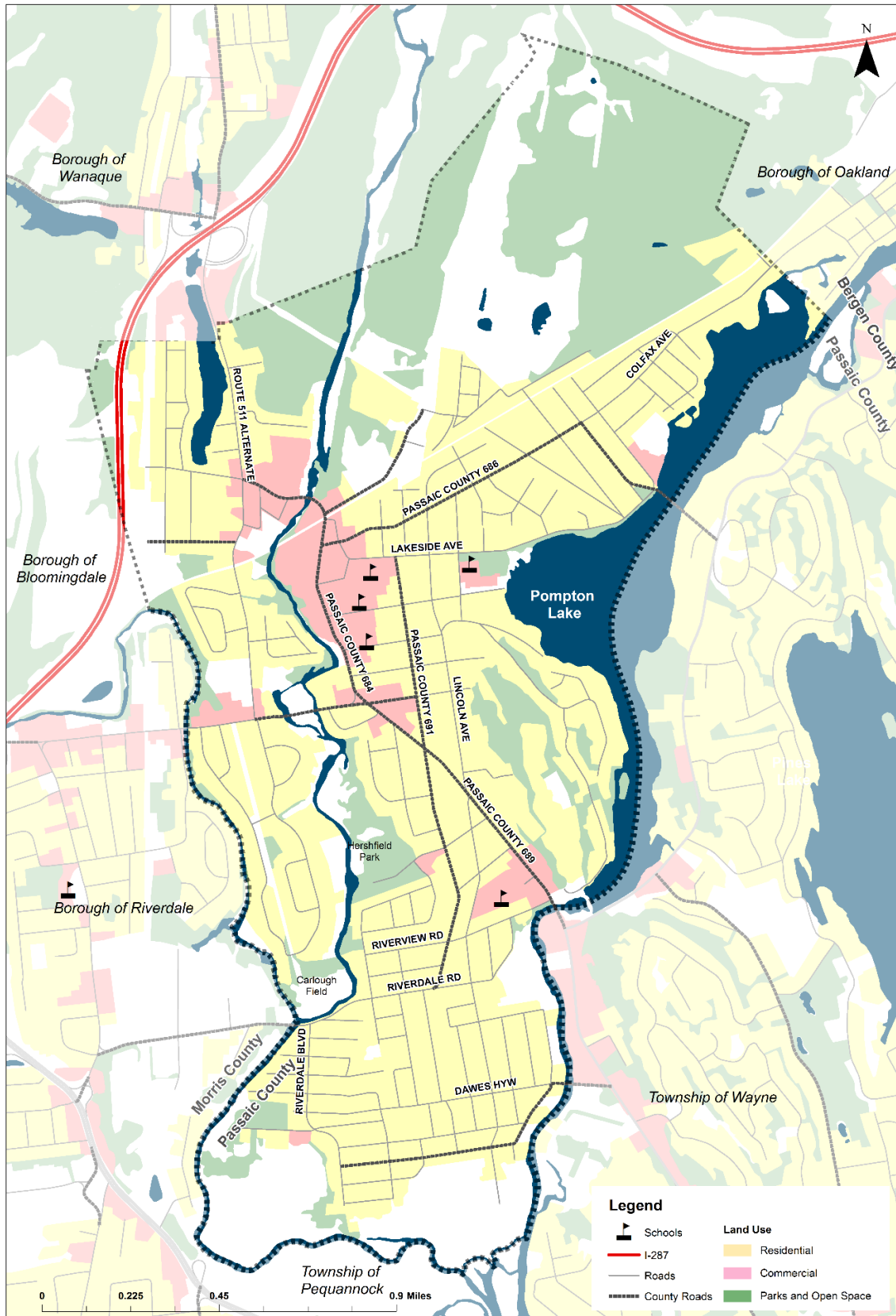
- Colfax Avenue
- Paterson-Hamburg Turnpike
- Ringwood Avenue
- Ramapo Avenue
- Wanaque Avenue

Municipal Roads

- Dawes Highway
- Lakeside Avenue
- Riverdale Boulevard
- Lincoln Avenue

County roads are under the jurisdiction for maintenance and improvements by Passaic County, while municipal roads are the responsibility of Pompton Lakes. In Figure 1 below the land uses within the Borough are identified along with County and Municipal Roadways.

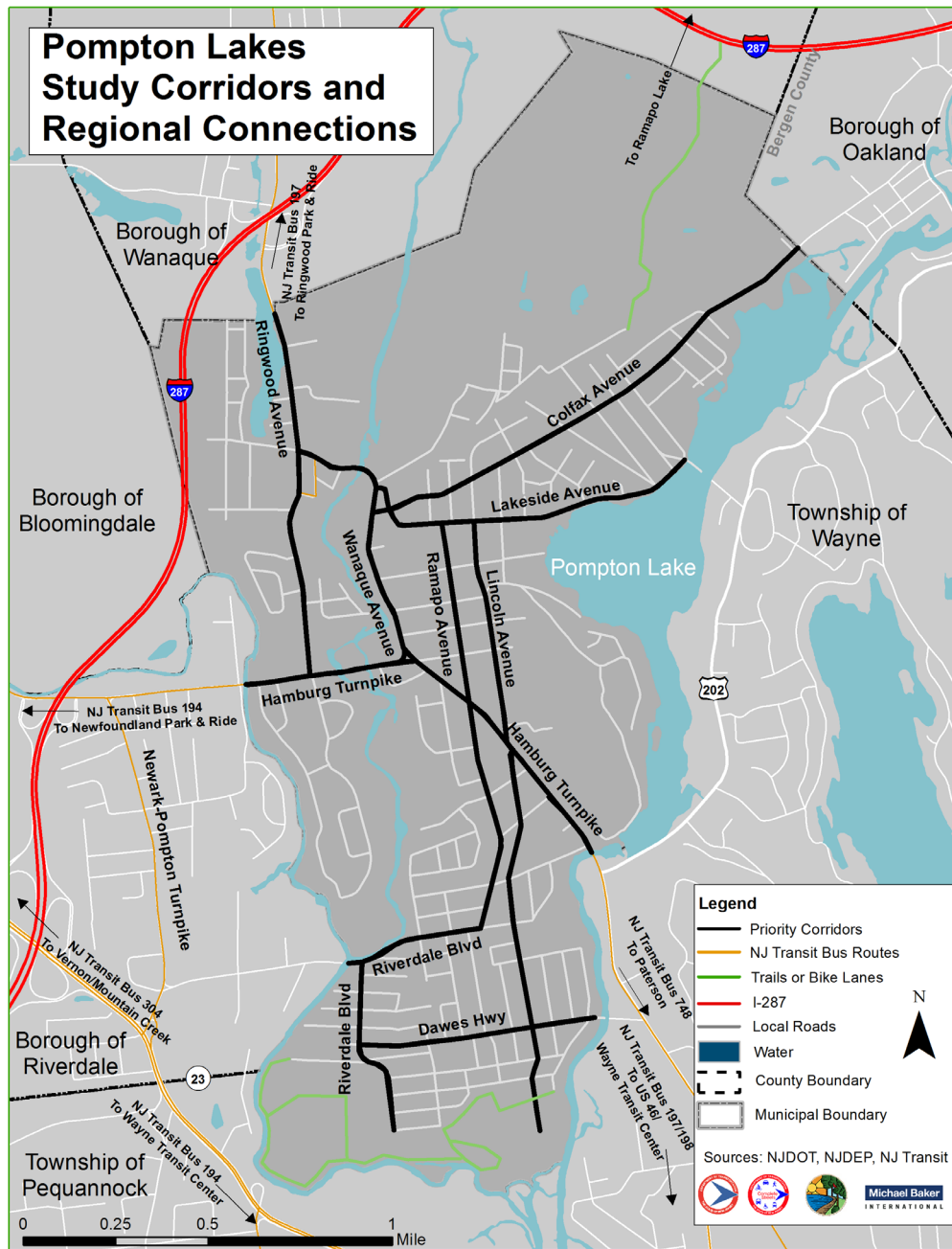
Figure 1: Land Use and Points of Interest



2.2 LOCAL AND REGIONAL CONNECTIONS

In keeping with the goals of the New Jersey Statewide Bicycle and Pedestrian Master Plan as well as the NJDOT Complete Streets Policy, which includes improving access to community destinations, transit facilities, and recreation facilities, accessible and convenient for use by all types and skill levels of bicyclists and pedestrians, the locations of existing, proposed, and planned pedestrian and bicycle facilities (including bicycle routes), shared use paths, and other active transportation networks in Pompton Lakes and adjacent communities were identified. This network is shown in Figure 2.

Figure 2: Pompton Lakes Study Corridors and Regional Connections



2.3 FIELD INVENTORY

To identify potential deficiencies that need to be addressed or opportunities to improve bicycle and pedestrian infrastructure during the development of the Complete Streets Implementation Plan, the project team performed an assessment of the existing conditions of Pompton Lakes. The data collected includes sidewalk conditions and widths, bicycles compatibility, intersection geometry and signal timings, crash frequency and locations, road widths, and speed limits. The following sections summarize the results of the data collected.

2.4 BICYCLE AND PEDESTRIAN CRASH ANALYSIS

Bicycle and pedestrian crash reports were obtained for the most recent five-year period available. Based on the most recent five-year (2012-2016) crash data available, there were 19 crashes in the Borough of Pompton Lakes that involved pedestrians (seven crashes) or bicyclists (twelve crashes). These 19 crashes are shown in Figure 3.

The largest concentration of pedestrian and/or bicycle crashes at a single location occurred at the intersection of Wanaque Avenue and Ringwood Avenue, with a total of four (21%) crashes. Crashes were predominantly concentrated along commercial corridors:

- Eight (42%) crashes occurred on Wanaque Avenue (CR 684)
- Four (20%) crashes occurred at the intersection of Wanaque Avenue and Ringwood Avenue
- Three (16%) crashes occurred on Ringwood Avenue (CR 511A)
- Two (11%) crashes occurred on Lakeside Avenue, and
- Two (11%) crashes occurred on Paterson Hamburg Turnpike



Figure 3: Pompton Lakes Pedestrian and Bicycle Crash Locations

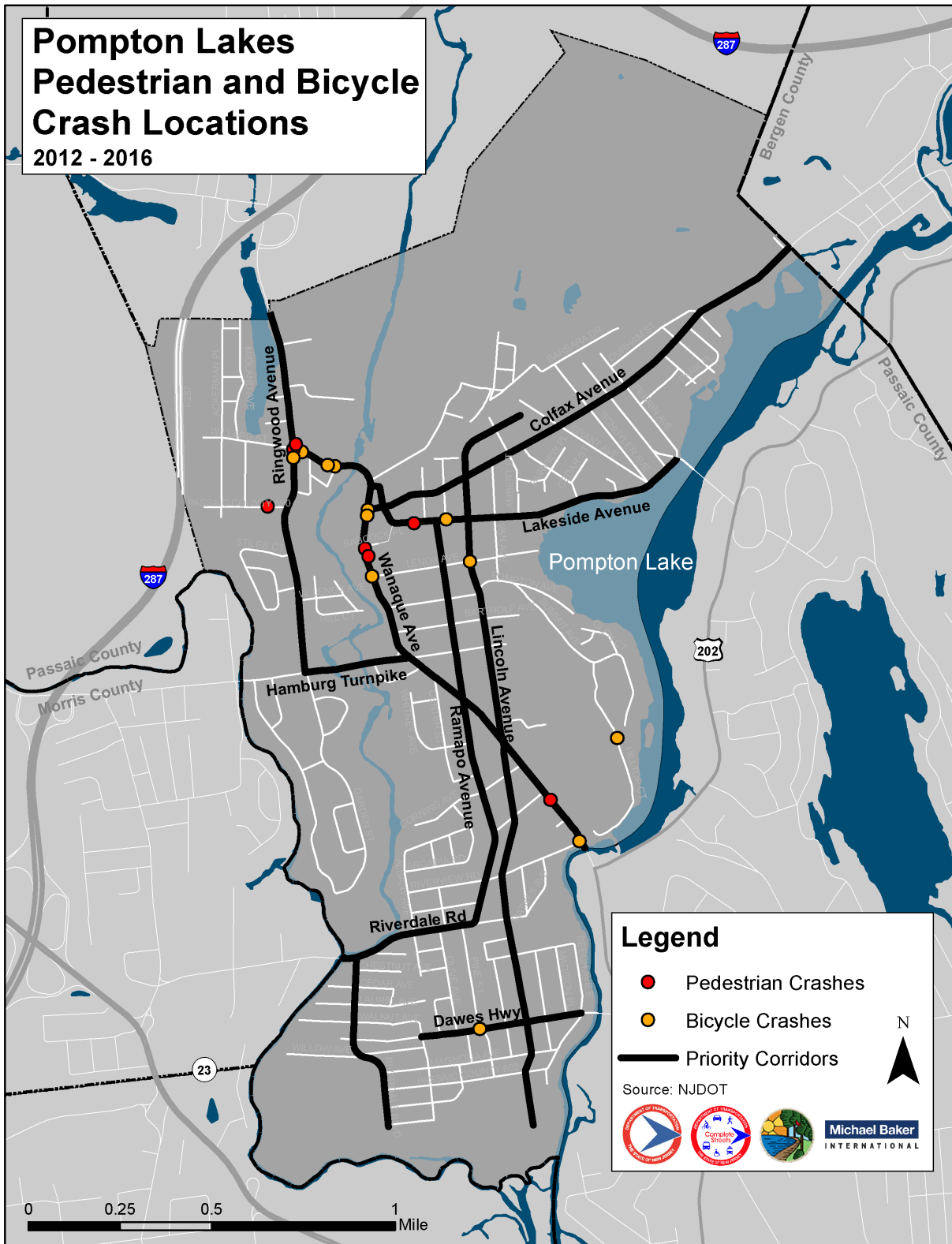


Figure 4 shows the pedestrian actions that occurred prior to the crash. 57% of pedestrians were Jaywalking prior to the crash occurring. 15% were crossing at a marked crosswalk, 14% were walking out from behind a parked vehicle and another 14% were walking two or from school.

Figure 4: Pedestrian Action Prior to Crash

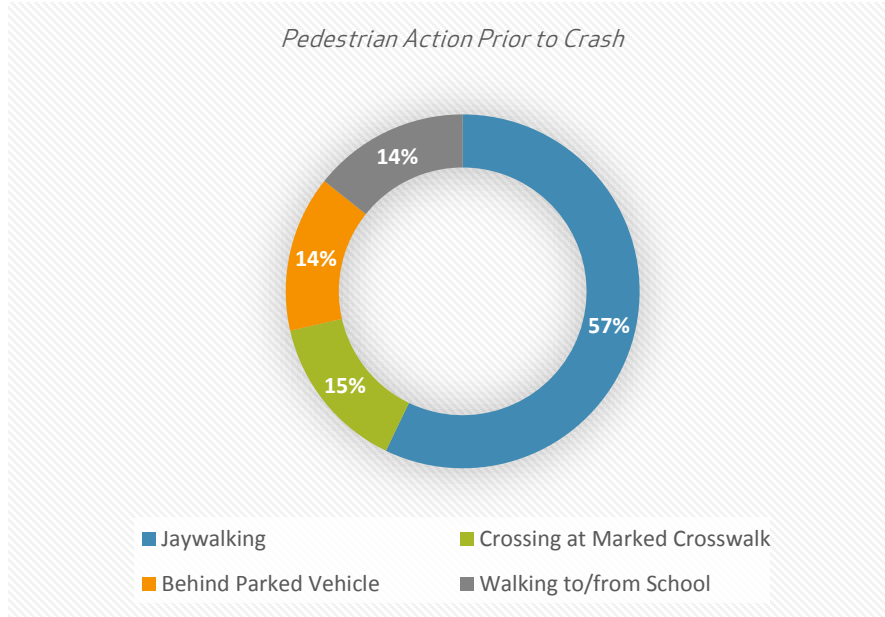


Figure 5 shows the contributing factors of pedestrian crashes. Based on the information provided, 38% did not have contributing factors, 37% was due to driver inattention, 13% involved unsafe speeds and 12% involved issues with brakes.

Figure 5: Contributing Factors of Pedestrian Crashes

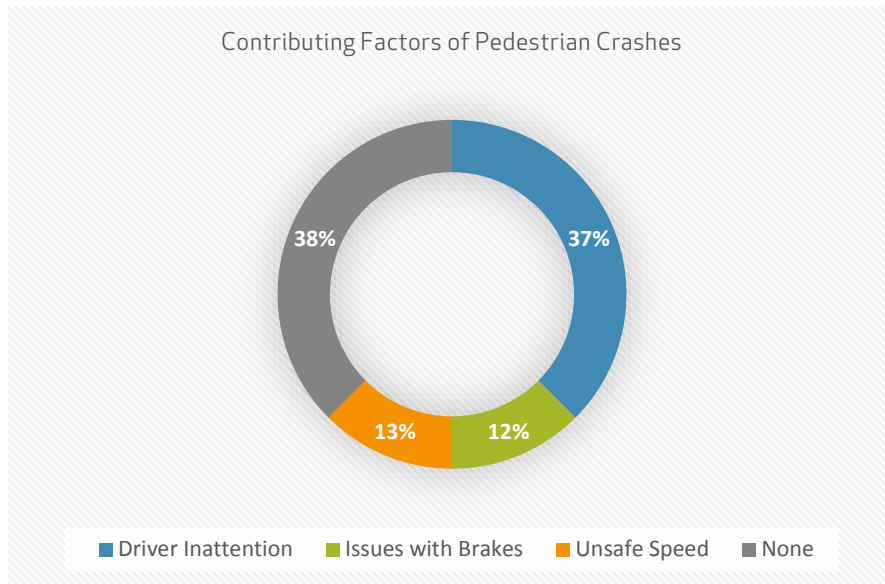


Figure 6 shows the bicyclist actions prior to the crash. Going straight ahead was the action taking place in 58% of the crashes reported. Other actions can be attributed to 25% and merging/entering traffic lanes is the remaining 17% of crashes.

Figure 6: Bicyclist Action Prior to Crash

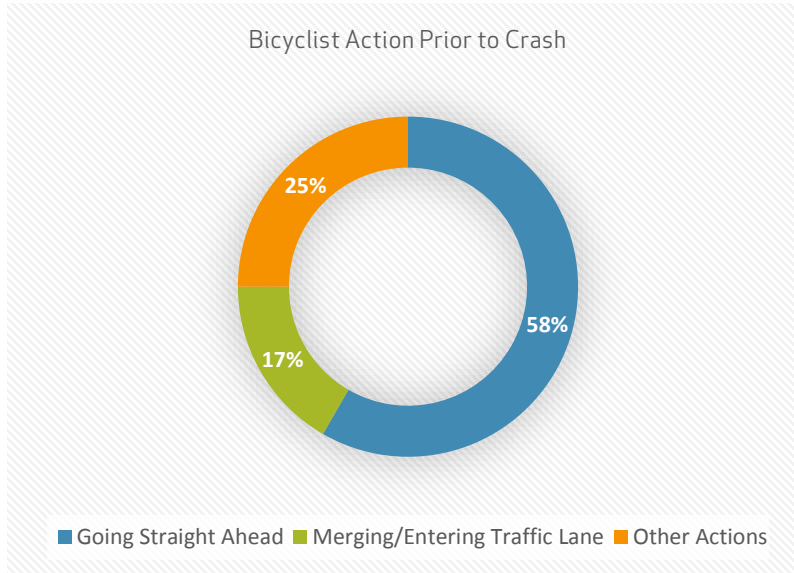
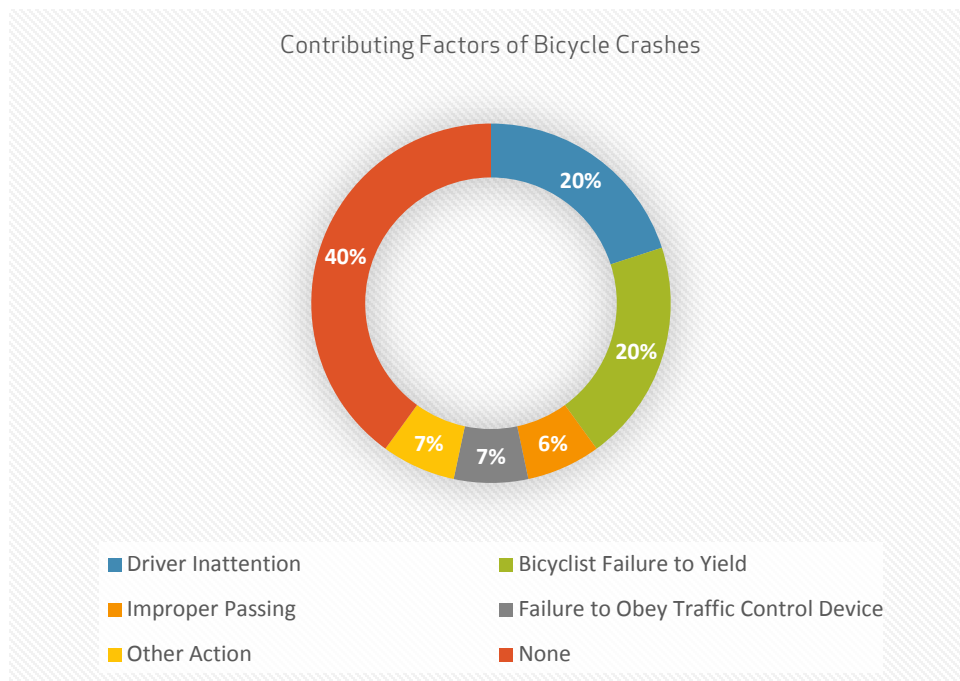


Figure 7 shows the contributing factors of bicycle crashes, 40% of the crashes did not have any listed contributing factors, 20% listed driver inattention and bicyclist failure to yield as contributing factors. Both Other actions and failure to obey traffic control devices made up 7% of the contributing causes and improper passing is the remaining 6%.

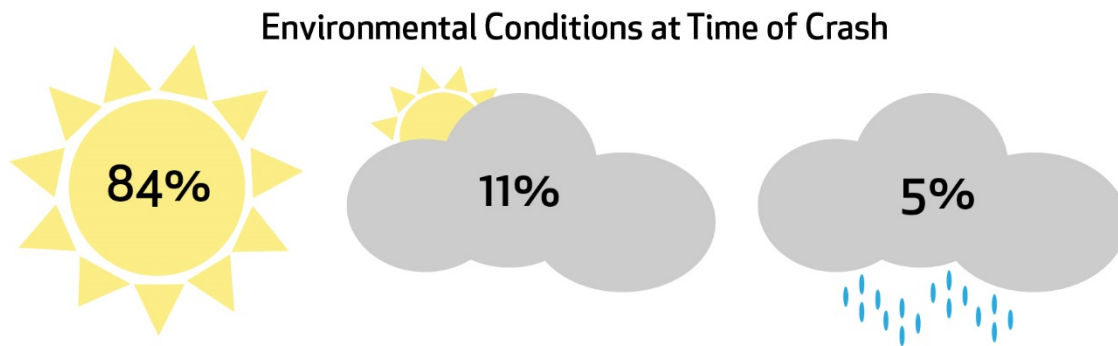
Figure 7: Contributing Factors of Bicycle Crashes



As seen in Figure 8 the majority of crashes occurred on clear days, under dry conditions, during the daytime. Of the 19 total crashes:

- ▶ Sixteen (84%) occurred during clear weather and dry surface conditions
- ▶ Two (11%) occurred during overcast weather and wet conditions
- ▶ One (5%) occurred in the rain

Figure 8: Environmental Conditions at Time of Crash



Additionally,

- ▶ Fifteen (79%) of the 19 crashes occurred during the day
- ▶ Two (11%) occurred in the darkness
- ▶ One (5%) occurred at dusk
- ▶ One (5%) occurred at dawn

In terms of severity, five (26%) of the 19 crashes resulted in moderate injury, nine (47%) of the 19 crashes resulted in minor injury, and five (26%) crashes resulted in property damage only (PDO). Overall, cyclist crashes were more likely to result in moderate injury, and pedestrian crashes were more likely to result in minor injury. Of the 12 crashes involving a cyclist:

- ▶ Four (33%) resulted in moderate injury
- ▶ Four (33%) resulted in minor injury
- ▶ Four (33%) resulted in PDO

Of the seven crashes involving pedestrians, one (14%) resulted in moderate injury, five (71%) resulted in minor injury, and one (14%) resulted in PDO.

The complete table can be found in Appendix E.

3. COMPLETE STREETS RECOMMENDATIONS

When developing recommendations for Complete Streets concepts, the Passaic County Complete Streets Guidelines was referenced for general roadway standards as many of the recommendations in this plan are located along Passaic County roads. Passaic County recommends that crosswalks shall be ADA compliant and signalized intersections shall include countdown pedestrian signal heads. At intersections within commercial districts and areas conducive to pedestrian traffic, Passaic County recommends that crosswalks should be properly signed and striped and the use of longitudinal thermoplastic stripes should be considered to delineate crosswalks. Where feasible, Passaic County recommends traffic calming measures to be implemented on the approaches to these walkable areas. Where deemed appropriate and necessary to calm traffic, Passaic County considers the use of roundabouts along County roads, but should only be considered along roads where they will operate safely and efficiently, and will not inhibit pedestrian access. Road Diets, curb extensions, shifting traffic lanes, and reduction of traffic lane width should be investigated for new and existing roads. Passaic County recommends that paved shoulders shall be provided on County roads between the travel lane and the streetside curb where possible to provide space for potential bicycle facilities.

Context-sensitive design recommendations were selected to fit the unique needs along the priority corridors and at specific intersections based on data collection and findings. This range of treatments were recommended to improve safety for all roadway users, calm traffic, improve pedestrian and bicycle safety access and mobility, and create a more livable, active community in Pompton Lakes.

The overall recommendations developed for this study are:

- **Install sidewalks where missing, particularly near schools and other points of interest.**
- **Provide bicycle facilities within the roadway network using bicycle lanes, “sharrows,” buffered bicycle lanes and shared use paths where appropriate. See Appendix B for concepts and recommended locations.**
- **Intersection improvements through traffic calming treatments, raised intersections and crosswalks and installation of roundabouts at recommended intersections. See Appendix C for concepts.**
- **Creation of trail connections.**
- **Wayfinding and Bike route designations.**

4. CURRENT PEDESTRIAN FACILITIES

Inventory of the existing sidewalk was conducted on the nine identified priority corridors. The inventory included areas with missing sidewalk, and sidewalk width and condition at locations where sidewalk is present. Table 1 shows the criteria that was used when determining the sidewalk condition and the completed inventory is illustrated in Figure 9.

Table 1: Sidewalk Condition Classifications

Value	Description
Good	New or nearly new material is present. No identifiable defects are present
Fair	Minor defects are present but are not considered detrimental to bicycle/pedestrian traffic
Poor	Major defects are present. Example: Sidewalk is severely cracked or is disintegrating. Bicycle/pedestrian travel could be difficult
No Sidewalk	No sidewalk present

Based on the sidewalk inventory, sidewalks largely exist throughout the priority areas and are mostly in good condition. There are small sections along Paterson Hamburg Turnpike, Lincoln Avenue, Ramapo Avenue, Colfax Avenue, and Ringwood Avenue with sidewalk sections which are in poor condition. Additionally, there are sections with no sidewalks along Dawes Highway, Lincoln Avenue, Riverdale Boulevard, Wanaque Avenue, Colfax Avenue, Lakeside Avenue, and Ringwood Avenue. Residents of Pompton Lakes showed the greatest concern with the missing sidewalk on Ringwood Avenue.

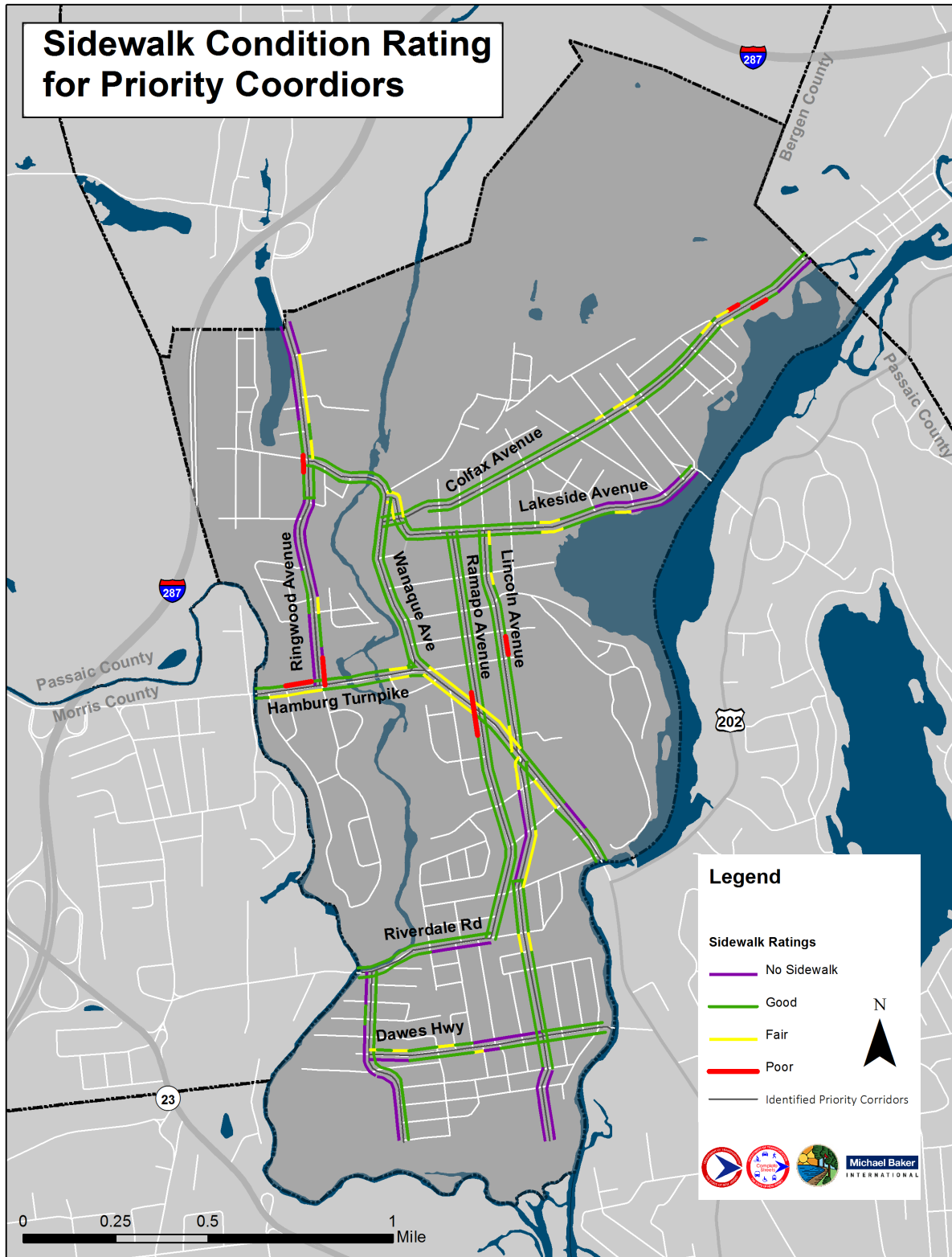


Colfax Avenue



Ringwood Avenue

Figure 9: Sidewalk Condition Rating for Priority Corridors



5. PEDESTRIAN FACILITY IMPROVEMENTS

The majority of the corridors evaluated have sidewalks that are in fair to good condition. Several sections of the priority corridors assessed have sections of missing sidewalk which include:

- ▶ Ringwood Avenue
 - Paterson-Hamburg Turnpike to West Lenox Avenue
 - Stiles Court to Locust Street
 - North of Albert Street
- ▶ Colfax Avenue
 - Near Pierson Miller Drive
- ▶ Lakeside Avenue
 - Mandeville Street to Jefferson Avenue
- ▶ Paterson- Hamburg Turnpike
 - Southeast of Pequannock Avenue
- ▶ Lincoln Avenue
 - Corning Avenue to Riverview Road
 - South of Magnolia Avenue
- ▶ Riverdale Road/Boulevard
 - Ramapo Avenue to Albany Avenue
 - Riverdale Road to Cedar Avenue
 - South of Walnut Avenue
- ▶ Dawes Highway
 - Maple Avenue to Woodlawn Avenue
 - Olive Avenue to Lincoln Avenue

Ringwood Avenue



Lakeside Avenue



Lincoln Avenue



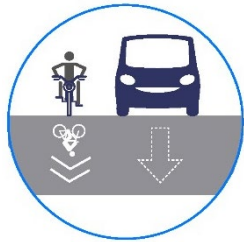
The Passaic County Complete Street Guidelines recommends that sidewalks be a minimum width of 4’ in residential areas, and in more developed areas 5’ or more. The sidewalk also should be continuous and be provided on both sides of the streets whenever possible. Therefore, installation of sidewalks is recommended at all missing locations of the pedestrian network. Priority should be placed on Lincoln Avenue due to Lincoln Elementary School.

Additionally, each intersection within the priority corridors were assessed for curb ramps and pedestrian signals. It is recommended that curb ramps be implemented where they are not present. The majority of intersections were compliant and curb ramps were present. Pedestrian signals are located at the four signalized intersections within the priority corridors.

6. BICYCLE FACILITIES

6.1 BICYCLE FACILITY TYPES

The New Jersey Complete Streets Design Guide outlines types of on-road bicycle facilities.



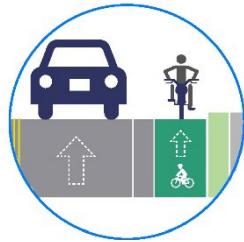
“SHARROWS”

Also known as shared lane markings, are used on roadways when it is not feasible to have a dedicated bicycle facility. Shared lane markings are used to show that the roadway environment is to be shared between bicycles and automobiles. These markings should not only show that the roadway is shared but should also suggest exactly where on the roadway the bicyclist should ride. This helps motorists to anticipate the presence of bicyclists.



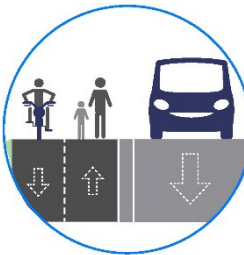
BICYCLE LANE

According to the New Jersey Complete Streets Design Guide, bicycles lanes provide an exclusive space for bicyclists using pavement markings and signage. It is the preference to paint these lanes green to draw awareness to them and further increase bicycle safety. Bicycle lanes are meant for one-way travel and typically are located on both sides of two-way streets and one side of one-way streets. Bicycle lanes can enable bicyclists to ride at their preferred speed, without interference from motorists.



BUFFERED BICYCLE LANES

Buffered Bicycle Lanes follow the same guidelines as typical Bicycle lanes, plus they include a marked buffer space that separates the bicycle lane from the adjacent travel lanes or parking lanes. Buffers decrease the risk of conflict between bicyclists and motor vehicles.



SHARED USE PATH

Shared use paths are similar to bike lanes however, they can be used by other modes of non-motorized transportation such as walking, running or skateboarding. They are also more distinctly separated from the roadway. Shared use paths should be located outside of the roadway pavement width, separated from traffic by either open space or a barrier. Unlike bike lanes, shared use paths are designed for two-way travel.

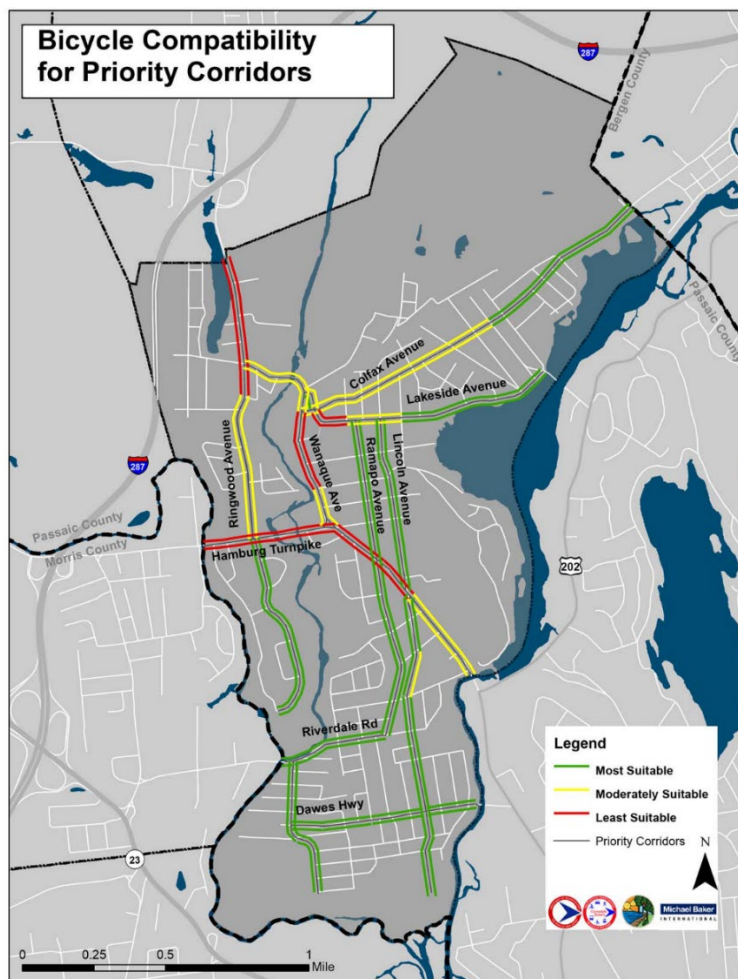
6.2 BICYCLE COMPATIBILITY ASSESSMENT

There are currently no dedicated bicycle facilities along the nine priority corridors. The corridors were inventoried using Bicycle Compatibility Rating Criteria tables to determine the suitability of the streets for on-road cycling. The classifications are listed in Table 2. The Bicycle Compatibility Rating Criteria tables use roadway geometry, speed limits, AADT and shoulder presence to determine if a roadway is most suitable, moderately suitable or least suitable for bicycles. While a roadway may appear as Most Suitable according to the criteria it is still important to ensure that there are pavement markings or signage to inform vehicles on the roadway of bicycle presence. Therefore, many of the roadways marked as Most Suitable will still have improvement recommendations. The Bicycle Compatibility Rating Criteria tables are included in Appendix D. Figure 10 shows the current suitability of each priority corridor for bicyclists.

Table 2: Bicycle Compatibility Classifications

Value	Description
Most Suitable	Most suitable for on-road cycling for users of all skill levels. Most cyclists would find conditions favorable
Moderately Suitable	Moderately suitable for on-road cycling. Cyclists of lesser skill and experience may find conditions unfavorable
Least Suitable	Least suitable for on-road cycling. Cyclists of advance skill and experience riding in traffic may find conditions unfavorable

Figure 10: Bicycle Compatibility for Priority Corridors



7. BICYCLE FACILITIES IMPROVEMENTS

The implementation of bicycle facilities is a critical step towards encouraging cycling in an area as an essential form of transportation. However, it is important that bicycle facilities are properly designed to ensure that they are safe, comfortable and useful to most people. According to the New Jersey Complete Streets Guidelines there are five guiding principles to achieve effective implementation:

- **Continuous:** many bicycle lanes disappear at intersections and other stressful locations. To be successful, bicycle lanes must be continuous through these locations.
- **Connected:** Gaps in a bicycle network can discourage potential riders. Bicycle routes should be interconnected to create a robust network that connects where people live and where they want to go.
- **Convenient:** Bicycle networks must conveniently and directly connect cyclists to key destinations to encourage higher rates of cycling.
- **Complete:** A successful network considers what happens when a bicycle ride ends. This means considering how complete a street is, including the presence of sidewalks, bicycle parking, and access to transit
- **Comfortable:** A bicycle network should be comfortable and inviting for riders of all ages and abilities, providing the sense that cycling is a safe and convenient activity.

7.1 RECOMMENDED BICYCLE FACILITIES

The recommended bicycle facilities are proposed to improve bicycle compatibility and accessibility in Pompton Lakes. They include a variety of bicycle facility treatments such as "sharrows," bicycle lanes, buffered bicycle lanes, and shared use paths. The recommendations are intended to be implemented within the existing cross-section of the roadway as part of re-surfacing, restriping or other roadway reconstruction projects.

Bike lanes are a common on-road bicycle facility and there are several opportunities to install them on roadways throughout Pompton Lakes. In locations where sufficient roadway width is available; a buffer could be included between the bike lane and the parking lane or between the bike lane and the travel lane. A buffer may be preferred by less skilled cyclists when higher traffic volumes and speeds are present.

In locations where space is constrained, and bike lanes cannot be accommodated, sharrows are proposed. Sharrows may be used to indicate a shared environment for bicycles and automobiles. Under these conditions, it is recommended that either "Share the Road" signs and Shared Lane Markings be incorporated to reinforce the shared lane concept.

The following table outlines the recommended bicycle facilities and roadway conditions necessary for their recommendation. These recommendations follow the guidelines set in both the New Jersey Complete Street Design Guidelines and the Passaic County Complete Streets Design Guidelines. Additional bicycle facilities with design guidance can be found in the New Jersey Complete Streets Design Guide. The proposed cross section alternatives for each priority corridor along with a map of the specific locations can be found in Appendix B. Many of the locations have several alternatives, guidance from Passaic County and the State of New Jersey should be utilized to determine which alternative will be best for Pompton Lakes.

Table 3: Bicycle Facility Criteria Table

Facility Type	Facility Width	Roadway Speed Limit (MPH)	Average Daily Traffic (ADT)
Bicycle Lane	5'	25-35	< 10,000
Buffered Bicycle Lane	5' Bike Lane 3' Buffer	25-45	< 15,000
"Sharrow"	-----	≤ 25	< 10,000
Shared-Use Path	10-14'	Any	Any

8. SIGNALIZED INTERSECTIONS

The signalized intersections along the nine priority corridors were assessed to determine if the existing traffic signal timings, including pedestrian crossing times and pedestrian signal operations are in conformance with signal timing directives, NJDOT guidelines, and guidance from the Manual of Uniform Traffic Devices 2009 Edition. There are four signalized intersections along the nine priority corridors. Existing and proposed intersection signal timings are shown in Appendix I. Based on the signalized intersection assessments, the following deficiencies were identified:

8.1 RINGWOOD AVENUE AND PATERSON-HAMBURG TURNPIKE

Based on assessment of the signal timings for the intersection of Ringwood Avenue and Paterson Hamburg Turnpike, the Flashing Don't Walk (FDW) phase for crossing Paterson-Hamburg Turnpike should be increased to 15 seconds.

8.2 RINGWOOD AVENUE AND WILLARD STREET/WANAQUE AVENUE

The yellow change interval for the southbound lead phase on Ringwood Avenue should be increased to four seconds. Additionally, the all red interval should be increased to four seconds, as well, for the eastbound and westbound Wanaque Avenue phase.

8.3 WANAQUE AVENUE AND GROVE STREET/SHOPPING CENTER

The existing timings for this intersection are adequate and do not require adjustments.

8.4 PATERSON HAMBURG TURNPIKE AND RAMAPO AVENUE

The all red interval should be increased to four seconds for the Ramapo Avenue phasing, as well as for the pedestrian phase crossing Paterson-Hamburg Turnpike. No turn on red from Paterson-Hamburg Turnpike onto Ramapo Avenue.

9. INTERSECTION IMPROVEMENTS

Intersections are one of the most critical parts of any transportation network. They are key points for all users as they travel through a street network and can act as important nodes of activity for community life. While they can have positive impacts on community life they also account for the most serious and frequent conflicts between all travel modes. If an intersection is not function properly, it can dramatically reduce mobility and safety for all modes. However, a well-designed intersection that facilitates visibility and predictability for all users can reduce crashes. Intersection design should allow the street space to be effectively shared by pedestrians, bicyclists, and drivers. There are several different strategies that can be used to achieve this goal.

The Passaic County Complete Streets Guidelines encourage pedestrians to cross at signal-controlled intersections. These signalized intersections should be properly delineated for pedestrian crossings. Additionally, crosswalks must be ADA compliant and signalized intersections should include countdown pedestrian signal heads. At intersections within commercial districts (along Wanaque Avenue) and areas conducive to pedestrian traffic, crosswalks should be properly signed and striped, and the use of longitudinal thermoplastic stripes should be considered to delineate crosswalks. Along with signage and striping, signalized intersections should have lighting and where feasible, traffic calming measures. The following are different strategies and traffic calming measures recommended throughout Pompton Lakes.

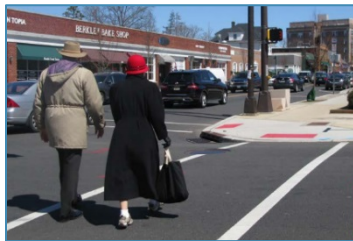
9.1 HIGH VISIBILITY CROSSWALKS

A crosswalk is a portion of a roadway designated for pedestrian to cross streets. The striping of crosswalks is important, it creates a high level of visual contrast with the surface of the roadway to draw both pedestrian and drivers attention. Some striping styles are more visible than others. It is recommended that Pompton Lakes use a ladder style striping or red brick paver crosswalks. These have been shown to be the most visible and are recommended in the Passaic County and New Jersey Complete Streets Guidelines.



Source: Michael Baker

9.2 CURB RAMPS



NJ Complete Streets Design Guide

ADA guidelines require appropriately designed curb ramps at all pedestrian crossings. These curb ramps are essential to provide easy access at crossings for pedestrians of all ages and abilities. Curb ramps assist in providing a smooth transition from the sidewalk level to the street level and back again. Additional to the curb ramp, detectable warning surfaces should also be included. These warning surfaces assist people with visual impairments determine safe crossing locations.

9.3 CURB EXTENSIONS

Curb Extensions are an example of a traffic calming measure. These can also be referred to as bulb-outs or bump-outs. A curb extension extends the curb line and sidewalk into the existing roadway, thus expanding the available pedestrian realm. The benefits of curb extensions include the following:

- **Increased visibility for pedestrians and drivers**
- **Reduction of pedestrian crossing distance**
- **Traffic calming**
- **Shields on-street parking from intersection**
- **Expands pedestrian realm**



<http://www.whiteflint.org/tag/curb-extensions/>

9.4 PEDESTRIAN REFUGE ISLANDS



Also referred to as Crossing Islands in the New Jersey Complete Streets Design Guide, pedestrian refuge islands provide traffic calming benefits and increased pedestrian safety at intersections. They enable pedestrians to make a crossing in two parts, giving them the ability to stop in the middle. This reduces the exposure time of pedestrians to vehicular traffic.

9.5 RAISED CROSSINGS AND INTERSECTIONS

Raised crossings and intersections increase driver awareness to the presence of pedestrians. They force vehicular traffic to slow down as they pass through the crossing or intersection. This strategy has also been shown to increase the rate at which motorists comply with the “stop for pedestrians law” per the New Jersey Complete Streets Design Guide. Raised crossing and intersections work best when applied on minor streets with access to major pedestrian destinations, such as routes to school.



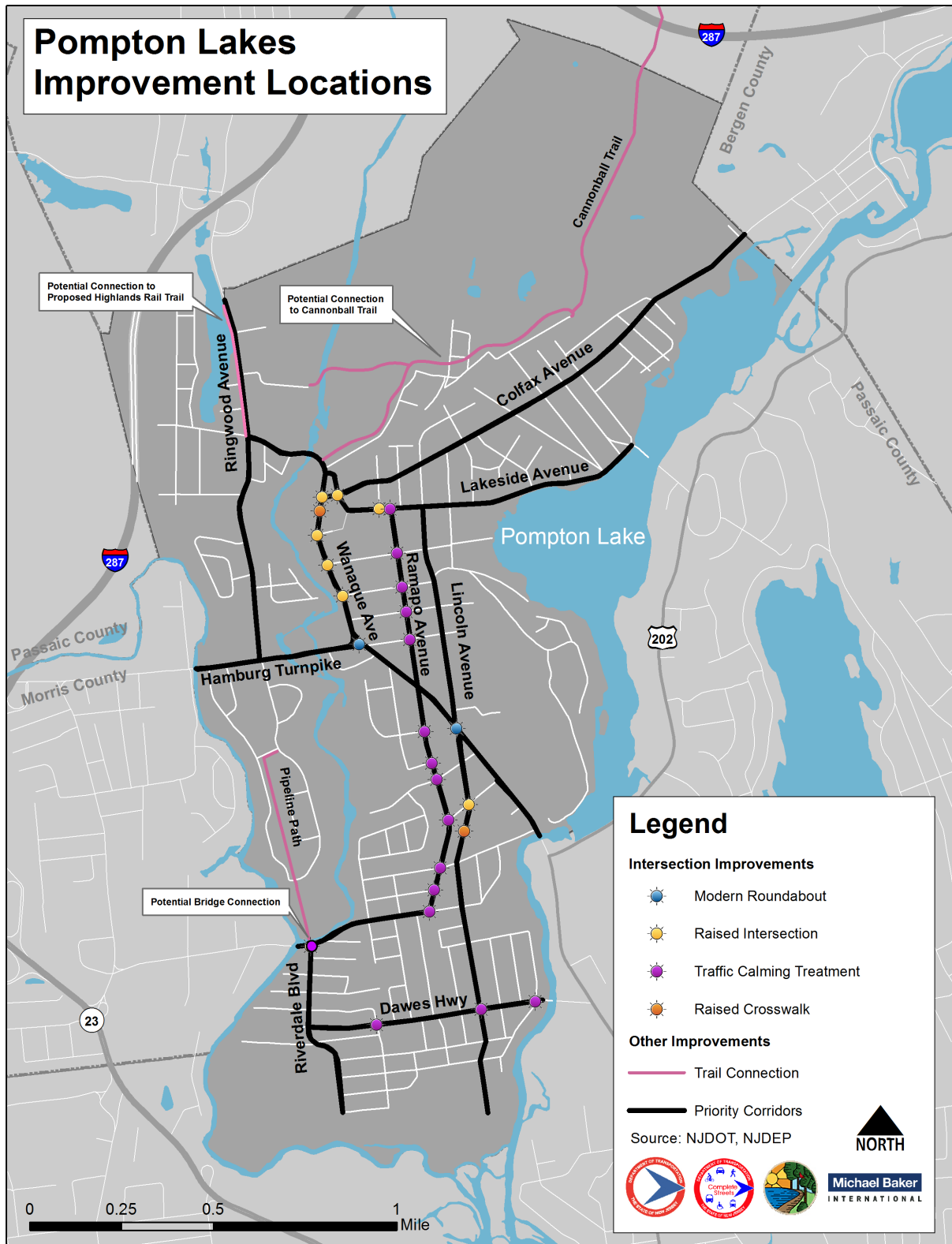
10. RECOMMENDATIONS

Specific recommendations include two intersections that were assessed for roundabouts, two mid-block crossing improvements were recommended, seven raised intersections and fifteen intersections that need traffic calming treatments. The recommended improvements are listed in Table 4 and are shown in Figure 11.

Table 4: Intersection Improvement locations

Recommended Improvement	Intersection
Roundabout	Wanaque Avenue and Passaic Avenue
	Paterson-Hamburg Turnpike and Lincoln Avenue
Raised Intersections	Wanaque Avenue and Colfax Avenue
	Colfax Avenue and Lakeside Avenue
	Lakeside Avenue and Adrian Street
	Wanaque Avenue and Babcock Place
	Wanaque Avenue and Lenox Avenue
	Wanaque Avenue and Pompton Avenue
	Lincoln Avenue and Pequannock Avenue
Traffic Calming Treatments	Ramapo Avenue and Lakeside Avenue
	Ramapo Avenue and Lenox Avenue
	Ramapo Avenue and Pompton Avenue
	Ramapo Avenue and Bartholf Avenue
	Ramapo Avenue and Passaic Avenue
	Ramapo Avenue and Whitney Avenue
	Ramapo Avenue and Edwin Place
	Ramapo Avenue and Hershfield Park Place
	Ramapo Avenue and Riverview Road
	Ramapo Avenue and Watervliet Ave
	Ramapo Avenue and Riverdale Road
	Dawes Highway and Woodlawn Avenue
	Dawes Highway and Lincoln Avenue
	Dawes Highway and Riveredge Drive
Riverdale Road and Riverdale Boulevard	
Raised Crosswalks	Wanaque Avenue between Colfax Avenue and Babcock Place
	Lincoln Avenue at Lincoln Elementary School

Figure 11: Pompton Lakes Improvement Locations



The intersection of Ringwood Avenue and Wanaque/Willard Avenue was noted as the intersection with the highest frequency of pedestrian crashes. The following is a list of recommendations specific to this intersection that should be implemented to prevent further crashes. These improvements are also shown in Figure 12.

- 1) Installation of retroreflective backplates.
- 2) Upgrade from 8" to 12" LED signal heads.
- 3) Consideration of No Turn on Red signage.
- 4) Implementation of a Leading Pedestrian Interval with countdown timers
- 5) ADA compliance at the Intersection and High visibility crosswalks.
- 6) Sidewalk improvements at the southwest corner.

Figure 12: Proposed Intersection Improvements at Ringwood Avenue and Wanaque/Willard Avenue

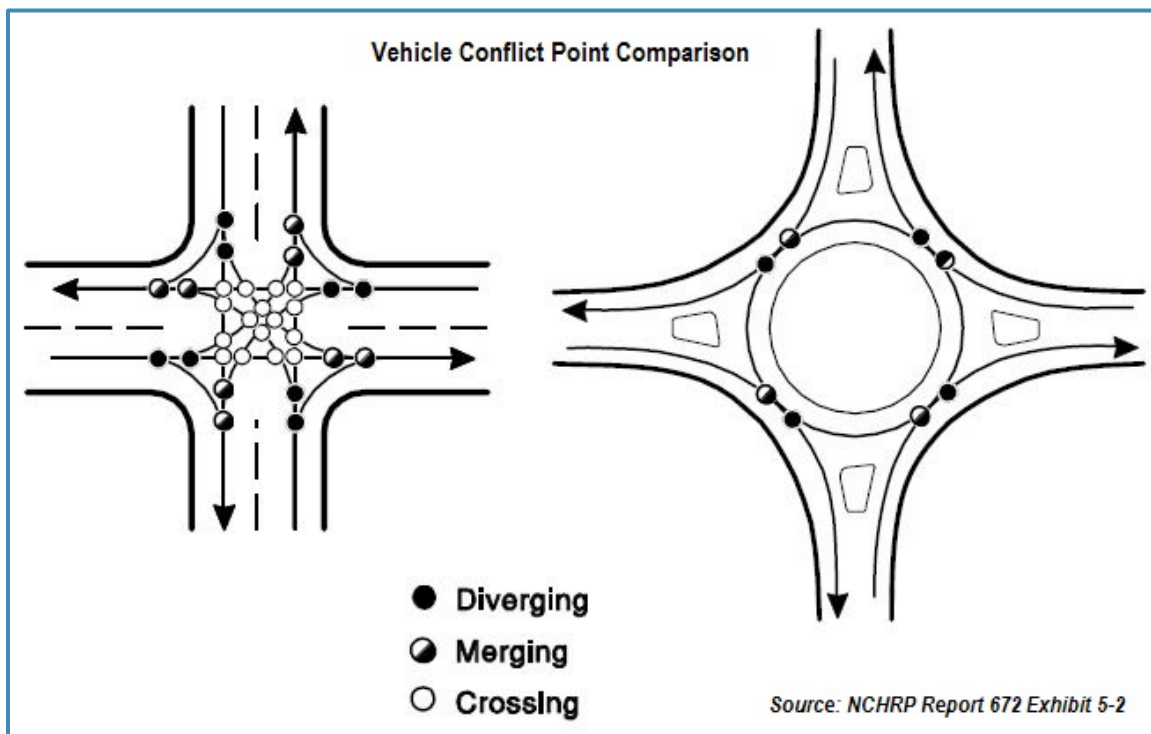


10.1 ROUNDABOUTS

Recently the modern roundabout has been recommended as an alternative to traditional signalized intersections for several reasons. The primary reason to recommend a modern roundabout is because they have been shown to reduce the frequency of certain crash types as well as reduce crash severity. Within acceptable parameters, roundabouts provide better operational performance than a signal in terms of stops, delay, vehicle queues, fuel consumption, safety, and pollution emissions (Source: Roundabout Informational Guide, FHWA). Additionally, roundabouts have been recommended for implementation in Pompton Lakes at busier intersections where changing traffic control would reduce crashes and improve walkability. The safety benefits of modern roundabouts are attributed to several factors:

- ▶ A reduced number of vehicle conflict points. There are 32 conflict points for a conventional intersection and only eight conflict points for an equivalent roundabout. An illustration of these conflict points can be seen below in Figure 13.

Figure 13: Vehicle Conflict Point Comparison



- ▶ Reduced number of crashes. Speeds in roundabouts are generally slower giving motorists more time to assess potential conflicts and react. Reduces right angle and head-on crashes.
- ▶ Reduces crash severity. The lower speeds entering roundabouts reduce crash severity. Right angle and head-on crashes typically create the most severe injury types.
- ▶ Safer pedestrian crossings. There are also a reduced number of vehicle/pedestrian conflict points. With a pedestrian refuge island, pedestrians cross one direction of traffic at a time.

Roundabouts have also been shown to improve operations of roadways. This can be attributed to the fact that vehicles do not need to come to a complete stop when traveling in a roundabout, therefore vehicles typically

experience less delay than in other types of intersections. Additionally, roundabouts do not have as many on-going maintenance costs when compared to signalized intersections.

The Passaic County Complete Streets Guidelines states, “where deemed appropriate and necessary to calm traffic, the use of roundabouts shall be considered along County roads.” In Pompton Lakes four intersections were studied for modern roundabout improvements. These intersections are two- or four way stop controlled. Below in Figures 14 and 15 proposed Roundabouts at two intersections within the Borough are shown.

Figure 14: Proposed Roundabout at Paterson-Hamburg Turnpike and Lincoln Avenue

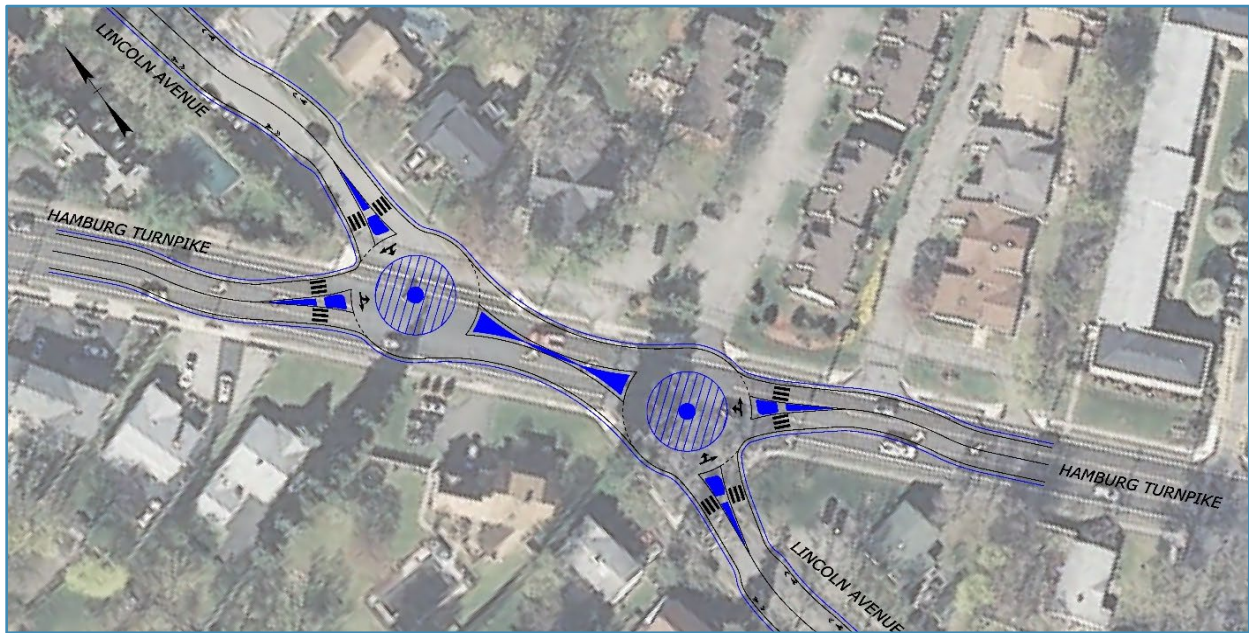
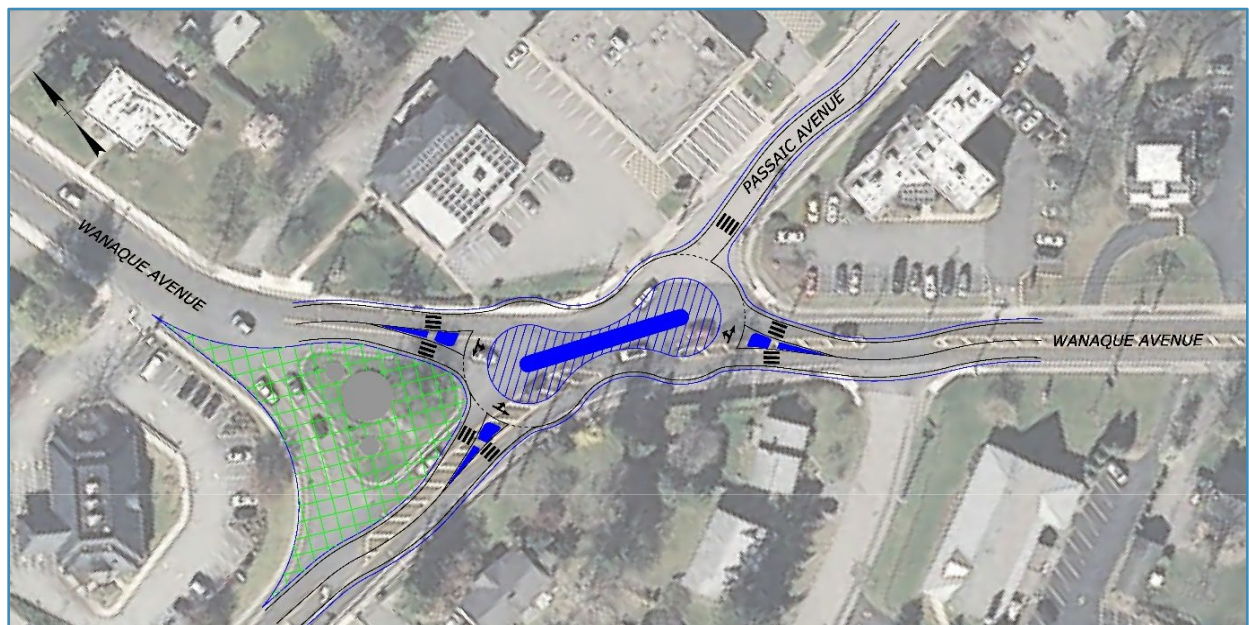


Figure 15: Proposed Roundabout at Wanaque Avenue and Passaic Avenue

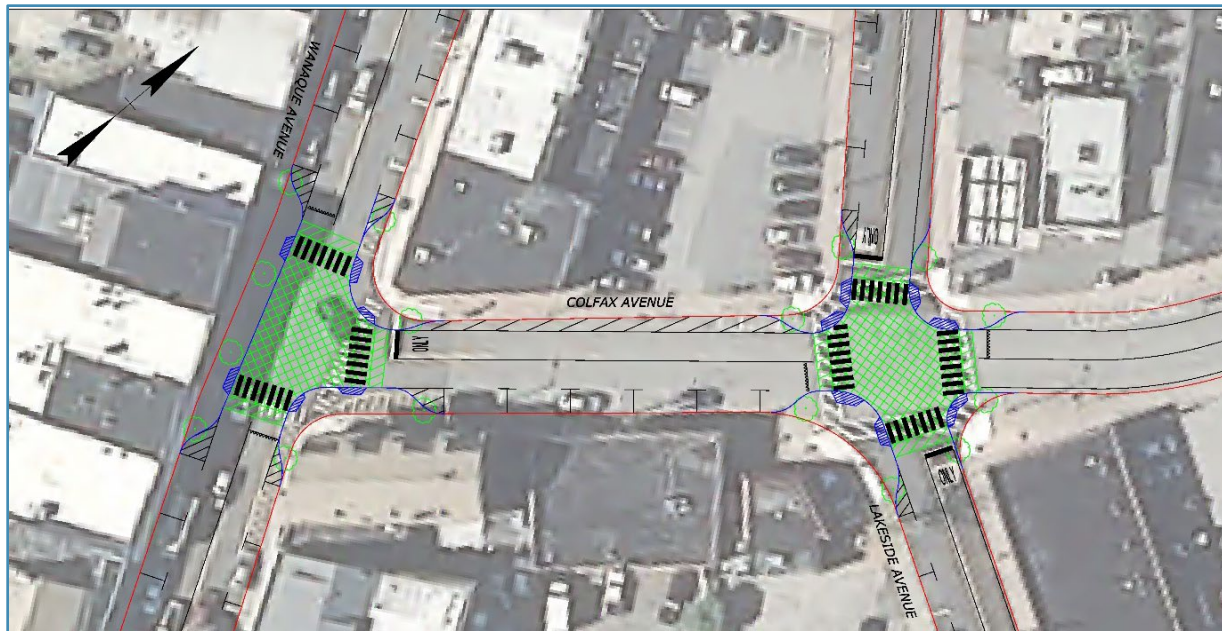


Appendix J shows diagrams of the most recent three years of crashes that occurred at intersections with proposed roundabouts.

10.2 RECOMMENDED RAISED INTERSECTIONS AND CROSSINGS

Two mid-block crossings and seven raised intersections have been recommended to Pompton Lakes. An example of two proposed raised intersections are shown in Figure 16. Additionally, the raised intersections include curb extensions, new curb ramps, new crosswalks, and street trees.

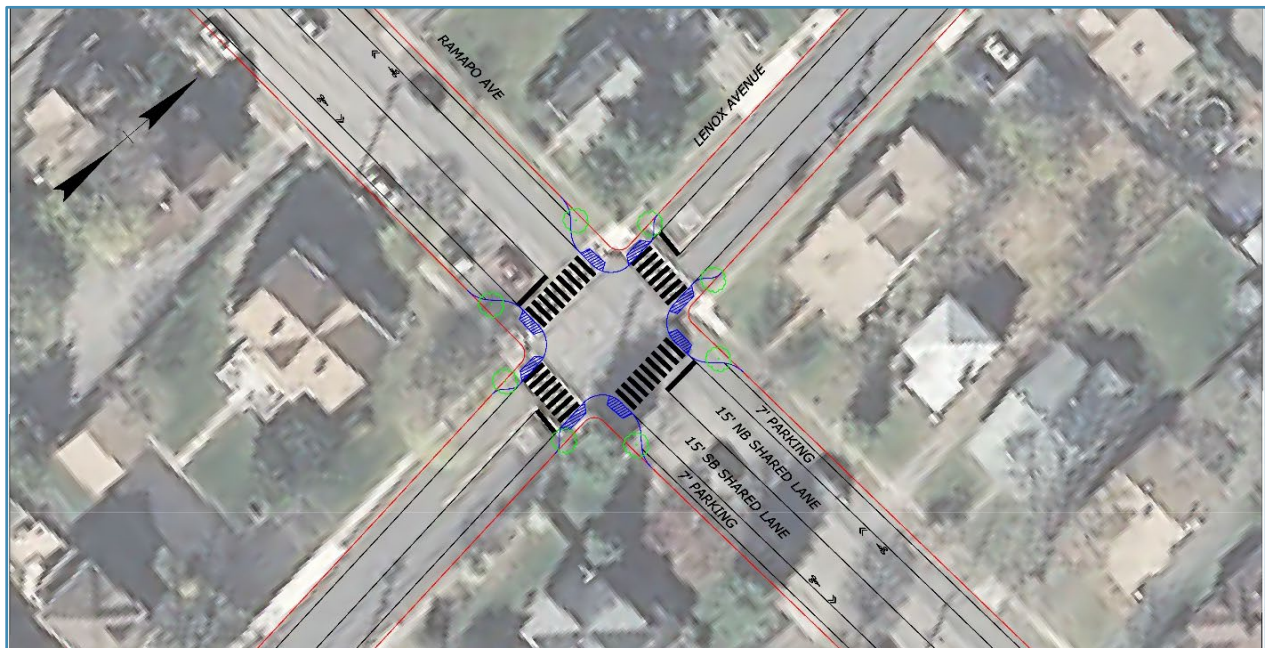
Figure 16: Raised Intersections- Wanaque Avenue and Colfax Avenue, Colfax Avenue and Lakeside Avenue



10.3 RECOMMENDED TRAFFIC CALMING TREATMENTS

There are several strategies of traffic calming treatments. A preferred treatment for Pompton Lakes are curb extensions. Curb extensions improve the safety and quality of the pedestrian environment by extending the curb line and sidewalk into the existing roadway. Curb extensions improve visibility at intersections and reduce the pedestrian crossing distance while also calming traffic. The extra pedestrian environment space can be used for placemaking opportunities such as installation of street furniture. It also allows for flexibility in placement of curb ramps, lighting, traffic signal control equipment and signage. An example of a traffic calming treatment at the intersection of Ramapo Avenue and Lenox Avenue is shown in Figure 17. The proposed treatments at this intersection include curb extensions, new curb ramps, new crosswalks, street trees, and shared use markings in the travel lanes.

Figure 17: Example of Traffic Calming Treatments-Ramapo Avenue and Lenox Avenue



10.4 FUTURE RECOMMENDATIONS

The Steering Committee recommended to eliminate one of two Center Street entrances on the south quadrant of the intersection of Paterson-Hamburg Turnpike and Center Street. Once this entrance is removed, the recommendation is to reconfigure the remaining entrance to a 90 degree angled connection with Paterson-Hamburg Turnpike. This recommendation should be evaluated in a future implementation plan.

11. TRAIL CONNECTIONS

There are two existing trails within the project boundary, Pipeline Path and Cannonball Trail. Additionally, the proposed Highlands Rail Trail could potentially connect to Ringwood Avenue in Pompton Lakes. The recommendations for each are detailed below.

11.1 PIPELINE PATH

Pipeline Path connects Cross Street south to Carlough Field. The recommendation is to extend the path through to the intersection of Ringwood Avenue and Cross Street to the east. To connect the path to Riverdale Boulevard, a pedestrian bridge over the Wanaque River should be evaluated. Completing this recommendation would assist in creating a complete circuit for the bike route that is being developed. It would also increase the pedestrian access from the northern area of Pompton Lakes to the southern area as an alternative to using Paterson-Hamburg Turnpike.

11.2 CANNONBALL TRAIL

Cannonball Trail connects to Pompton Lakes from Barbara Drive located north of Colfax Avenue. The path then travels north over Interstate 287 to Ramapo Lake. The existing trail should be formalized and extended to make Cannonball Trail more visible and accessible from Pompton Lakes. The trail can utilize Walnut Street/Dupont Place and Cannonball Road to connect to Wanaque Avenue. Additionally, Barbara Drive can provide connections to Colfax Avenue via Schuyler Avenue. As those routes are mainly residential, installing a bicycle facility should be explored.

11.3 PROPOSED HIGHLANDS RAIL TRAIL

The Passaic County Highlands Rail Trail is a proposed recreational trail that would reuse the former New York & Greenwood Lake Railway. This trail would provide a pedestrian and bicycle-friendly pathway through northern Passaic County. Currently the proposed recreational trail would not enter Pompton Lakes. However, if a shared-use path was implemented on the northern portion of Ringwood Avenue, it would be an opportunity to connect Pompton Lakes to this future trail.

12. WAYFINDING

An important step in advertising and promoting the facility improvements being made to these priority corridors are wayfinding signage. The National Association of City Transportation Officials (NACTO) defines a bicycle wayfinding system as comprehensive signing and/or pavement marking that guide bicyclists to their destinations along preferred bicycle routes. Typically, signs are placed at decision points along bicycle routes, this could be at intersections of other major locations. Wayfinding signage helps to make less experienced bicyclists more comfortable in the environment and encourage these bicyclists to use the safest routes available.



Wayfinding systems can be implemented and designed formally by a municipality or business improvement district. However, in many cases walking and biking advocates have organized informal wayfinding systems.

Benefits:

- Familiarize cyclists with the bicycle network
- Identify preferred routes to key destinations
- Increase awareness of the bicycle network to drivers
- Increases accessibility and convenience of the bicycle network to visitors and casual users
- If mileage and/or travel time information is included it can minimize the tendency to overestimate the amount of time necessary to travel to a destination

The New Jersey Complete Streets Design Guide outlines some design guidance for wayfinding:

- Signage should maintain a clean, visible, and consistent design
- Signs should be on both sides of the street or trail
- Maps should be properly oriented so that the direction the user is facing is at the top
 - A “You Are Here” symbol should be included
- Distances should be provided by the time needed to reach the destination

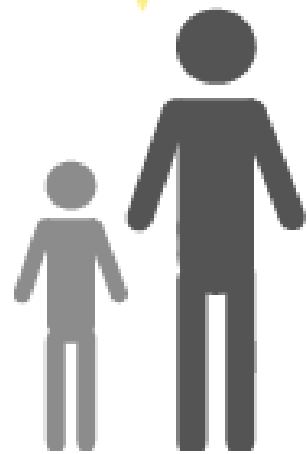
13. PUBLIC OUTREACH

Public Outreach to engage with the individuals that live, work, and visit the project area is essential to developing an understanding of the community and the improvements that are needed. The Public Outreach for this project included the organization of a Steering Committee and Public Information Centers. Additionally, Michael Baker developed a survey and crowdsourcing map, both of which were openly available to the public.

13.1 PROJECT STEERING COMMITTEE

A Steering Committee was formed of local, county and state officials, as well as other stakeholders identified by the NJDOT-OBPP and Pompton Lakes. The Steering Committee assisted with identifying deficiencies and opportunities for active transportation facilities within Pompton Lakes and provided feedback on potential improvements. Representatives of the following offices, organizations and constituencies were invited to participate:

- Pompton Lakes Planning Board
- Pompton Lakes Borough Council
- Pompton Lakes Mayor
- Pompton Lakes Borough Administrator
- Passaic County Planning Department
- North Jersey Transportation Planning Association
- TransOptions TMA
- Pompton Lakes Business Improvement District
- Police Department representatives
- School District Representatives
- Department of Public Works



After a project kick-off meeting, two Steering Committee meetings were held. The first Steering Committee meeting took place on February 8, 2018 to present key findings of data collected and assessed. Additionally, the Steering Committee confirmed the chosen priority corridors and added additional streets. The second meeting was held on July 12, 2018 to present draft conceptual improvements and bicycle and pedestrian facility recommendations. Comments and input received during these meetings guided the improvement alternatives. Summaries from both meetings are included in Appendix H.

13.2 PUBLIC INFORMATION CENTERS

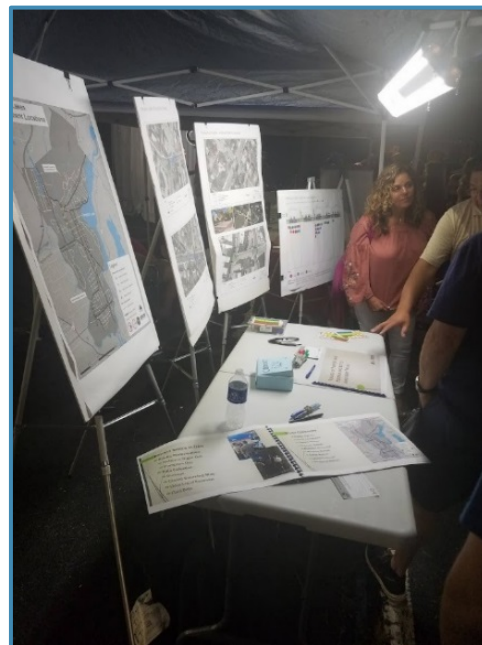
A Public Information Center (PIC) is an opportunity for any member of the public to speak with the project team, voice opinions and give feedback. Public Information Centers can be a meeting scheduled and advertised on its own or it can be in combination with a different public event already occurring. For this project, the PICs took place at Pompton Lakes National Night Out. Photos from National Night out in 2017 and 2018 are shown in Figures 18 and 19 respectively.

The first PIC was held during Pompton Lakes National Night Out on August 1, 2017. Representatives from Michael Baker and the Borough were in attendance to introduce the project, present preliminary findings and to gain participation and information from the public. A Public Outreach Survey was developed to compile public opinion as well as determine bicycle and pedestrian activities within Pompton Lakes. This survey was distributed at the first PIC. Paper copies of the survey, as well as electronic versions (provided on laptops) were available for the public to fill out. The survey results are discussed in the following section and a copy of the survey is included in Appendix F. Maps of Pompton Lakes were provided for the public to mark up with input regarding what they perceive as problem locations. Project information and surveys were also made available to the public during Pompton Day, September 3, 2017.

Figure 18: National Night Out 2017



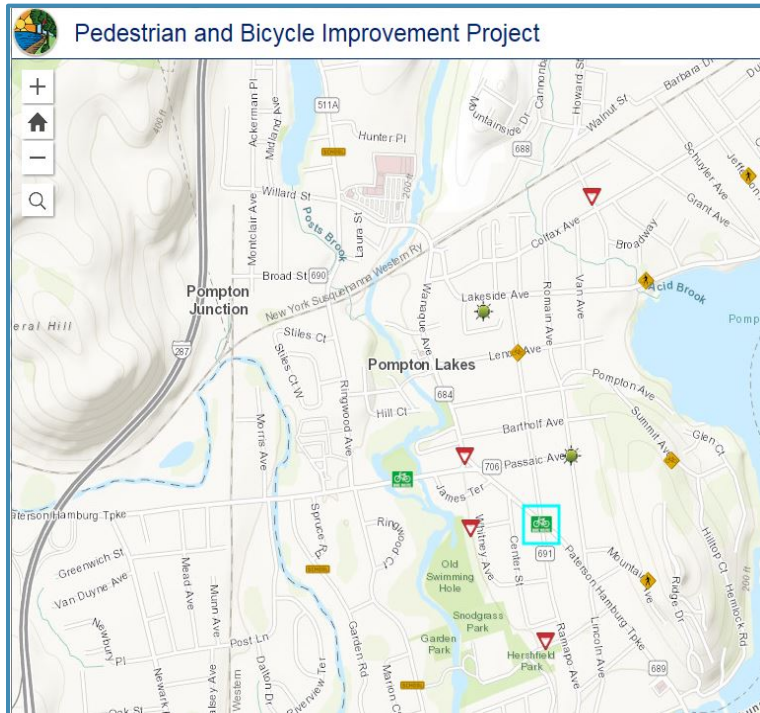
Figure 19: National Night Out 2018



The second PIC was held the following year at National Night Out on August 7, 2018. Members of the project team presented the conceptual improvements developed for bicycle facilities, pedestrian connectivity and intersections. A preference survey displayed on a poster board and comment cards were utilized to solicit community feedback at the event. Through the comment cards, the project team was able to obtain positive feedback about the Roundabout concepts presented. Additionally, the project team was able to explain the purpose and benefits of roundabouts to those that attended. The poster board is included in Appendix G.

13.3 PUBLIC OUTREACH SURVEY

Figure 20: Crowdsourcing map



Public Outreach surveys and an Online Crowdsourcing map were used to solicit concerns, opportunities, needs and desires of the community. Both techniques enabled the project team to better understand the goals of Pompton Lakes and receive crucial input and feedback from those that live within the project area. Since both the survey and map were available online, people were able to contribute to the study at any time. The Crowdsourcing map gave the public the ability to point out specific locations of concern and comment on the conditions that are present. Figure 20 shows an image of what the crowdsourcing map looked like once responses had been submitted.

In total, 176 completed surveys were received. During National Night Out 2017 and the period immediately after the event, 95 completed surveys were received. Pompton Lakes promoted this project on their website again in January 2018 and February 2018, which resulted in an additional 35 and 46 completed surveys respectively.

Based on the data collected in the summer, 73% of those surveyed walked or biked at least a few times a week. With the additional winter results, 65% of those surveyed walked or biked at least a few times a week. The summer data and winter data showed that a total of 83% and 82% of those surveyed walked or biked at least once per month, respectively.

Over 50% of those surveyed, responded that they walk or bike to do errands, go to the park, visit friends, and go out to eat sometimes or often. Over 30% walk or bike to work or school sometimes or often. And over 20% walk or bike to a transit stop sometimes or often. Of those surveyed in the summer, 92% had walked more than a block within the last week. With the additional winter results, 82% had walked more than a block within the last week. The summer surveys showed that 58% of those surveyed had biked within the last month, with over 43% biking at least within the last week. The additional winter survey results showed that over 49% of those surveyed had biked within the last month. While over 29% had biked within the last week. These results demonstrate that Pompton Lakes is a very active multimodal town.

Over 74% of those surveyed are concerned about missing and insufficient sidewalk in Pompton Lakes, while 66% are concerned about speeding and aggressive driving. 55% are concerned about the insufficient space for bicyclists and over 30% are concerned about missing or insufficient crosswalks, and insufficient lighting. This information indicates

the areas of concern for the majority of residents and therefore shows where priority should be placed in implementing the improvements recommended in this report.

Of those surveyed, 52% view Pompton Lakes as safe for all road users and 58% feel children are safe walking to and from schools or parks. This leaves a large percentage of individuals feeling unsafe on the roadways in the Borough.

The locations listed below are where respondents to the surveys felt unsafe:

- ▶ **Lakeside Avenue – 43 Mentions**
- ▶ **Paterson-Hamburg Turnpike – 42 Mentions**
- ▶ **Wanaque Avenue – 40 Mentions**
- ▶ **Colfax Avenue – 40 Mentions**
- ▶ **Ringwood Avenue – 35 Mentions**

Some of the recommended improvements for Pompton Lakes from the surveys include:

- ▶ **Bicycle Lanes/Bicycle Paths**
- ▶ **Improved sidewalks/Additional sidewalks**
- ▶ **Additional Lighting**
- ▶ **Bicycle Racks**
- ▶ **Additional Crosswalks**
- ▶ **Removal of on-street parking**
- ▶ **Additional “Yield to Pedestrian” signs**
- ▶ **Bicycle Safety Courses**
- ▶ **More enforcement for speeding and distracted driving**
- ▶ **Roundabouts at Paterson-Hamburg Turnpike and Wanaque Avenue**
- ▶ **Speedbumps**
- ▶ **Radar Speed Signs**

CROWDSOURCING MAP

The crowdsourcing map was available to the public through the Pompton Lakes website. The map allows the project team to better understand the goals of Pompton Lakes and to receive input and feedback from those that live in the area and that may have been unable to attend the in-person public outreach opportunities.

When using the crowdsourcing map, individuals could choose from several categories of response type. Each response type corresponded to a specific symbol that would then appear on the map at the specified location. The responses that were received are as follows:



WALKING CONDITIONS

- “There are no footpaths/sidewalks on either side of Jefferson Avenue.”
- “Mountain Avenue”



WALKING ROUTE TO SCHOOL

- “On Ringwood Avenue between Albert Street and Kluge Street on the West side there is no sidewalk. Also, 511A has two lanes for no reason. Turn one into a walk/bike path, this would reduce the speed of cars coming into town. An accident occurred here with a child on a bike not too long ago.”

- “On Ringwood Avenue near Marion Court is used as a school route but is very dangerous for students.”
- “Garden Road is used as a route to school”



SAFETY CONCERN

- “Broadway, between Colfax Avenue and Beam Street is very dark.”
- “Intersection of Wanaque Avenue, Passaic Avenue, and Paterson-Hamburg Turnpike is very difficult for bicycles.”
- “There is a sink hole located off Whitney Avenue.”



BICYCLE OBSTACLE OR BARRIER

- “Summit Avenue”
- “A fence at the intersection of Ramapo Avenue and Lenox Avenue.”



BIKING CONDITIONS

- “Intersection of Ramapo Avenue and Paterson-Hamburg Turnpike has poor biking conditions.”
- “Paterson-Hamburg Turnpike.”



FREQUENT DESTINATION FOR WALKING OR BIKING

- “The High School, off Lakeside Avenue is the location of National Night Out.”
- “Starbucks at the Intersection of Passaic Avenue and Lincoln Avenue.”

Individuals using this crowdsourcing map could also “like” the feedback of others. However, this data is not reliable because there was no limit on the number of times one person could “like” any of the feedback.

14. IMPLEMENTING THE PLAN AND BUILDING COMMUNITY BUY-IN

The recommendations outlined in the Complete Streets Implementation Plan provide an opportunity to enhance biking and walking throughout Pompton Lakes. There are multiple opportunities to improve bicycle and pedestrian access and mobility. The following sections provide guidance on coordination, planning, education, and funding sources that can serve as a resource for advancing and implementing the proposed facilities throughout Pompton Lakes.

14.1 COORDINATION

Coordination between Pompton Lakes, neighboring communities, and Passaic County should be initiated to advance improvements for bicycle and pedestrian accommodations on roadways. A potential next step could be the formation of a working group (e.g., Complete Streets Task Force) to spearhead a public information campaign, and pursue opportunities and resources to support the design and implementation of facilities. The working group would be led by a Complete Streets “Champion” and could assist with advancing priority recommendations and build upon the preliminary network and regional connections identified in this plan, as well as, identify opportunities for improving biking and walking through future development. An alternative to creating a new working group, the tasks around this project could be implemented into the tasks of an existing group, such as the existing Environmental Protection Committee, Open Space Committee, or a Neighborhood Green Team in Pompton Lakes.

The working group should create partnerships within the Pompton Lakes community to advance the Complete Streets Policy and Implementation Plan. Within the community, businesses, private developers and neighborhood associations can be important allies in providing ongoing support. Partnerships with neighboring municipalities, Passaic County, and NJDOT can help to achieve consistency in design treatments for roads operated by different agencies.

14.2 EDUCATIONAL PROGRAMMING

To encourage safe use of existing and proposed facilities and more walking and bicycling trips, it is recommended that Pompton Lakes promote walking and bicycling and implement educational programs on best practices and safety. Education programs are recommended for all types of users of all ages. Efforts should be made to educate bicyclists, pedestrians and motorists on the rules of the road and how to safely share the road. Widespread education efforts can contribute to safer roadways for all. Encouragement is also needed to promote the spread of bicycling and walking as means of transport, recreation, and physical activity.

Safe Routes to School (SRTS) is a federally funded program with the goal of making it safer for students, including those with disabilities, to walk and bike to school. NJDOT provides funding to schools and communities to improve walking bicycling conditions to schools through a SRTS Infrastructure Grant Program. At the local levels, assistance to schools and communities with Non-Infrastructure Programs is provided by the New Jersey SRTS Resource Center and the eight Transportation Management Associations.

TransOptions is a non-profit Transportation Management Association located in Morris County that provides free Safe Routes to School (SRTS) programming to its service area, which includes Pompton Lakes and western Passaic County. TransOptions advocates for safe walking and biking to school for students K-12 with educational programs such as Walk to School events, Bicycle Learn-To-Ride's, and in-class activities that teach students the rules of the road. TransOptions also provides evaluation programs such as walkability audits and the development of School Travel Plans.

SRTS and TransOptions can provide an educational component of Complete Streets to students in Pompton Lakes schools. To set up programming, Pompton Lakes is encouraged to contact the Safe Routes to School Coordinator at TransOptions. Additional SRTS resources can be found on the New Jersey SRTS Resource Center website and the National Center for Safe Routes to School website.



In addition to SRTS TransOptions also provides programming for the Street Smart Campaign. Street Smart is a public education, awareness and behavioral change campaign. The program utilizes visibility enforcement, education and public awareness to address pedestrian safety issues. Street Smart is organized by the North Jersey Transportation Planning Authority (NJTPA), the goals of this initiative are below.

- Change pedestrian and motorist behavior to reduce pedestrian crashes, injuries and fatalities in New Jersey.
- Educate motorists and pedestrians about their roles and responsibilities in safely sharing the roadways.
- Increase enforcement of pedestrian safety laws.

For more information visit the TransOptions or NJTPA website.

14.3 PROMOTIONAL ACTIVITIES

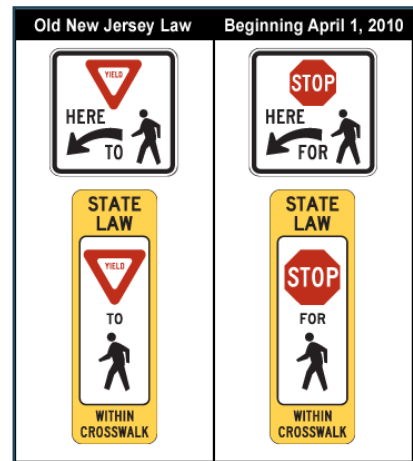
A wide variety of programs are available to encourage Pompton Lakes residents to walk or bicycle more often. Below are some recommended educational programs:

- **Walk to School Day:** This is one of the most fundamental strategies for encouraging younger residents to walk or bicycle. Although sometimes referred to as “Walk and Roll to School Day,” this event has been popularized in the past as “Walk to School Day,” and the typical focus has been on encouraging walking and biking to school. As one idea, walking and bicycling could be one of the units available in physical education classes. In the fall or spring, physical education teachers could enroll students in walking and bicycling events for a minimum number of miles. TransOptions can provide assistance in coordinating Walk to School events.
- **Join a Walking or Bicycling Club:** Residents of Pompton Lakes can start a club to encourage other residents to log on a certain number of miles per week on foot or bicycling.
- **Special Events:** A Walk to School Day is an example of a special event; other examples include Trails Day, Car Free Day, Traffic Safety Day, and Bike to Work Day. TransOptions holds an annual Bike to Work Challenge which Pompton Lakes residents are encouraged to participate in and potentially win prizes.
- **Awareness Campaign:** Public service announcements on cable television, posters, brochures, and bumper stickers promote increased use of walking or bicycling in general for errands, work trips, school and other purposes, or to promote special event days.
- **Commuter of the Month:** A Pompton Lakes business, public agency, or TransOptions could recognize the employee that walks or bicycles to work with the greatest frequency.

14.4 ENFORCEMENT

An important component of a safe and well-traveled transportation system is an enforcement program for traffic regulations as they apply to each type of roadway user: motorists, bicyclists, and pedestrians. Pompton Lakes can improve travel habits and behavior through enforcement. This process should include reviewing current ordinances and traffic regulations to identify elements that may unnecessarily affect certain roadway users, such as bicyclists. As bicycle facilities are installed, it is recommended that local ordinances and regulations be developed or revised to clarify items such as: application of vehicle laws to bicyclists, permitted movements on and across bicycle facilities (e.g., permitted motor vehicle movements across bicycle lanes), bicycling on sidewalks, and bicycle parking requirements.

In addition, a review of enforcement regulations and practices may assist in identifying opportunities to partner with community, county, or state organizations to inform users about safe bicycle travel behavior, such as the required use of helmets by bicyclists under the age of 17 (N.J.S.A 39:4-10.1), the N.J.S.A 39: 4-36 which requires motorists to stop for pedestrians in the crosswalk, or the N.J.S.A 39:4-14.2 which requires bicyclists to ride in single file. For more information on bicycle regulations in New Jersey, visit <http://www.state.nj.us/transportation/commuter/bike/regulations.shtm>



Outreach and promotion through community channels and events is a critical piece in reminding motorists, bicyclists, and pedestrians of applicable laws and recommended travel practices. The Street Smart campaign is one method that could be utilized.

14.5 CAPITAL IMPROVEMENT PROJECTS

Pompton Lakes should review their Capital Improvement Projects to determine where bicycle and pedestrian improvements can be integrated. The majority of bicycle facility recommendations outlined within this plan can be implemented as part of regular roadway resurfacing and/or restriping projects. When implemented as part of a larger maintenance or construction project the added cost for roadway markings and signage is minor within the scope of the larger project.

14.6 FUNDING THE IMPROVEMENTS

Several federal and state programs are commonly used to fund Complete Streets projects. Table 5 below provides a list of programs, the program administrator, who is eligible to apply, and the estimated amount of funding available for an individual allotment. Note: The estimated amounts are based on previous amounts awarded to municipalities and counties. Pompton Lakes can use this Implementation Plan to pursue funding through these programs.

North Jersey Transportation Planning Authority (NJTPA) works with its federal partners, NJDOT, its subregions and other state and local agencies to make travel safer and more reliable for all who use Northern New Jersey's transportation system. To support these efforts, NJTPA solicits candidate projects for implementation under two safety programs each fiscal year, the **Local Safety and High Risk Rural Roads** programs. These programs, which fund cost-effective solutions can make an immediate impact on their target areas.

The federally funded **Local Safety Program (LSP)** has allocated \$81 million since 2004 for quick-fix, high impact safety improvements. Projects supported by this program have included new and upgraded traffic signals, signage, pedestrian indications, crosswalks, curb ramps, pavement markings and other improvements to increase the safety of drivers, pedestrians, and bicyclists. LSP typically addresses NJTPA and/or NJDOT derived high priority crash locations on County or Local roads. The projects must be quick-fix, supported with detailed crash data and have minimal or no environmental or cultural resources impacts. The LSP funds the construction phase of work only, and therefore planning, design, and right-of-way acquisition are the responsibility of the sponsor.

Additional sources of funding include regional, county, local, or philanthropic organizations. Pompton Lakes can also pursue implementation of plan recommendations for locally-owned roads through their planning and engineering policies and roadway resurfacing programs. Adoption of the Complete Streets Policy by the Borough of Pompton Lakes will help implement Complete Streets when construction, reconstruction, and repaving projects are completed.

Many improvements (e.g., installing "Share the Road" or Wayfinding signage or striping a bike lane) can be implemented quickly and at a relatively low cost. There are a number of opportunities for grants to fund bicycle and pedestrian improvements. Additionally, the Borough should coordinate with Passaic County on county road projects that run through Pompton Lakes.

The recommended concepts for both bicycle and pedestrian projects could be eligible for the following potential funding sources:

NJDOT Municipal Aid – Each year NJDOT invites municipalities to apply for funds to go towards road improvement projects. This includes, resurfacing, rehabilitation or reconstruction and signalization. NJDOT has set a goal to award

up to 10% of the Municipal Aid program funds to projects such as pedestrian safety improvements, bikeways and streetscapes.

NJDOT County Aid – these funds are used for the improvement of public roads and bridges that are under county jurisdiction. Public transportation and other transportation projects are also included.

NJDOT Safe Routes to School – provides federal-aid highway funds for infrastructure projects that enable and encourage children in grades K-8, including those with disabilities, to safely walk and bicycle to school. Bonus points on the grant are given to applicants with School Travel Plans, a Complete Street Policy and Transit Village Designation.

NJDOT Safe Streets to Transit – provides funds to construct safe and accessible pedestrian linkages to transit facilities, to promote increased usage of transit by all segments of the population.

NJDOT Transportation Enhancements/ Transportation Alternatives Program – provides federal funds for community based “non-traditional” transportation projects designed to strengthen the cultural, aesthetic and environmental aspects of the nation’s intermodal system. Bonus points on the grant are given to municipalities that have an adopted Complete Street Policy and Transit Village Designation.

Sustainable Jersey – provides capacity building awards to municipalities to support local green teams and their programs to make progress toward Sustainable Jersey Certification.

New Jersey Healthy Communities Network – this is a partnership of grantees, funders, and advocate organizations who seek collective impact on a community’s well-being by supporting healthy eating and active living. The Community Grant Program provides the opportunity to develop healthy environments for people to live, work, learn and play by funding policies, projects and programs that support walking and biking.

New Jersey Transportation Bank – provides low interest loans to local government units for transportation infrastructure projects. It is a partnership between NJDOT and the New Jersey Infrastructure Bank (I-Bank).

NJDEP Recreational Trails Grant – The Federal Highway Administration’s Recreational Trails Program provides financial assistance to states for developing and maintaining trails and trail facilities. New Jersey Department of Environmental Protection administers the program in New Jersey.

Table 5: Funding Sources Matrix

Program Name	Program Administrator	Estimated Award (\$)	Eligibility	Additional Notes
<i>Local Safety Program</i>	NJTPA	N/A	Applies to local and county roads eligible for federal aid.	The project sponsor should demonstrate a safety need supported by crash data.
<i>Municipal Aid</i>	NJDOT	\$100,000 - \$500,000	Municipalities are eligible to apply for improvement of any public road or bridge governed by the municipality.	
<i>County Aid</i>	NJDOT	\$5 Million - \$10 Million	Counties are eligible to apply for improvement of public roads and bridges under county jurisdiction.	Each county must develop an Annual Transportation Program. Pompton Lakes should coordinate with Passaic County to list projects on county roads.
<i>Safe Routes to School</i>	NJDOT	Under \$500,000	Any county, municipality, school, school district, or board of education are eligible to apply.	Funds are intended to be used for projects that facilitate walking and/or bicycling to school.
<i>Safe Streets to Transit</i>	NJDOT	Under \$500,000	Counties and municipalities are eligible to apply.	
<i>Bikeway Grants</i>	NJDOT	\$100,000 - \$300,000	Counties and municipalities are eligible to apply	Funds support the State's goal of constructing 1,000 new miles of dedicated bicycle paths.
<i>Transportation Enhancements / Transportation Alternatives Program</i>	NJDOT	\$100,000 - \$500,000	Counties and municipalities are eligible to apply.	
<i>Sustainable Jersey Grants Program</i>	Sustainable Jersey	\$1,000 - \$35,000	Municipalities are eligible to apply.	
<i>NJHCN Community Grant Program</i>	New Jersey Healthy Communities Network	N/A	Municipalities, non-profit organizations, parks and recreation departments, school boards, are eligible to apply.	
<i>New Jersey Transportation Bank</i>	NJDOT and New Jersey Infrastructure Bank	N/A	Municipalities, counties, regional transportation authorities, or any other political subdivision of the state are eligible to apply.	
<i>Recreational Trails Grant</i>	New Jersey Department of Environmental Protection	Under \$50,000	Government agencies and non-profit organizations are eligible to apply.	

*The estimated award amounts are based on award amounts given in previous years.

14.7 IMPLEMENTATION MATRIX

It is recommended that Pompton Lakes determine a practical means for implementing the recommendations made in this plan. An Implementation Matrix for the proposed improvements is included as Table 6 to assist Pompton Lakes. The Implementation Matrix is intended to assist the Borough in prioritizing the recommendations for a phased implementation, as well as identifying costs and the appropriate agency to coordinate carrying them out. Prioritization is determined by the proximity of the locations to the Central Business District, Schools and other points of interest. It can also be affected by other projects that may be occurring in the same space, for example, a road resurfacing project, or new developments.

Table 6: Implementation Matrix

Timeframe:	Cost:	Priority Range:
Short = 1-2 years	Low = <\$10,000	Low
Medium = 3-4 years	Medium = \$10,000-\$250,000	Medium
Long = 5+ years	High = \$250,000+	High

Engineering Improvement	Location	Timeframe	Cost	Priority	Responsible Agency
Sidewalk Improvements	Areas of missing sidewalk	Medium	Medium	High	Dependent on location
Bicycle Facilities	Colfax Avenue	Short	Low	Medium	County & Pompton Lakes
	Lakeside Avenue	Short	Low	Medium	Pompton Lakes
	Lincoln Avenue	Short	Low	High	Pompton Lakes
	Ramapo Avenue	Short	Low	High	Passaic County
	Wanaque Avenue	Short	Low	High	Passaic County
	Ringwood Avenue	Short	Low	High	County & Pompton Lakes
	Paterson Hamburg Turnpike	Short	Low	Low	Passaic County
	Dawes Highway	Short	Low	Low	Pompton Lakes
Raised Crosswalk	Riverdale Boulevard	Short	Low	Low	Pompton Lakes
	Wanaque Ave between Lakeside Ave and Colfax Ave	Short	Medium	High	Passaic County
Raised Intersection	Lincoln Avenue at Lincoln Elementary School	Short	Medium	High	Pompton Lakes
	Adrian Street and Lakeside Avenue	Medium	Medium	High	Pompton Lakes
	Colfax Avenue and Lakeside Avenue	Medium	Medium	High	Pompton Lakes
	Colfax Avenue and Wanaque Avenue	Medium	Medium	High	Passaic County
	Wanaque Avenue and Babcock Place	Medium	Medium	High	Passaic County
	Wanaque Avenue and Lenox Avenue	Medium	Medium	High	Passaic County
	Wanaque Avenue and Pompton Avenue	Medium	Medium	High	Passaic County
	Lincoln Avenue and Pequannock Avenue	Medium	Medium	High	Pompton Lakes
Riverdale Road and Riverdale Boulevard	Medium	Medium	Low	Pompton Lakes	

Engineering Improvement	Location	Timeframe	Cost	Priority	Responsible Agency
Traffic Calming Treatment	Ramapo Avenue and Lakeside Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Lenox Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Pompton Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Bartholf Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Passaic Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Whitney Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Edwin Place	Short	Low	Medium	Passaic County
	Ramapo Avenue and Corning Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Pequannock Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Riverview Road	Short	Low	Medium	Passaic County
	Ramapo Avenue and Watervillet Avenue	Short	Low	Medium	Passaic County
	Ramapo Avenue and Riverdale Road	Short	Low	Medium	Passaic County
	Dawes Highway and Woodlawn Avenue	Short	Low	Low	Pompton Lakes
	Dawes Highway and Lincoln Avenue	Short	Low	Low	Pompton Lakes
	Dawes Highway and Riveredge Drive	Short	Low	Low	Pompton Lakes
Modern Roundabout	Paterson-Hamburg Turnpike and Lincoln Avenue	Medium	High	High	Passaic County
	Wanaque Avenue and Passaic Avenue	Medium	High	High	Passaic County
Trail Connection	Cannonball Trail	Long	High	Low	Passaic County
	Pipeline Path	Long	High	Low	Pompton Lakes
Wayfinding/Bike Route Designations	Designated Bike Route	Short	Low	Medium	Pompton Lakes
Policy Recommendation	Location	Timeframe	Cost	Priority	Responsible Agency
Adopt Complete Streets Policy	Borough Wide	Short	Low	High	Pompton Lakes
Educational Programming	Location	Timeframe	Cost	Priority	Responsible Agency
Safe Routes to School	Pompton Lakes School District/Borough Wide	Short	Low	High	TransOptions/Pompton Lakes School District
Encouragement	Location	Timeframe	Cost	Priority	Responsible Agency
Create Complete Streets Working Group	Borough Wide	Short	Low	High	Pompton Lakes
Awareness Campaign	Borough Wide	Short	Low	High	Pompton Lakes

15. NEXT STEPS

- **Adopt the Complete Streets Policy.**
- **Install sidewalks where missing.**
- **Provide bicycle facilities within the roadway network using bicycles lanes, “sharrows,” buffered bike lanes, and shared-use paths where appropriate. See Appendix B for concepts and recommended locations.**
- **Implement traffic calming measures at recommended intersections. See Appendix C for concepts.**
- **Install raised intersections and raised crosswalks at recommended intersections. See Appendix C for concepts.**
- **Upgrade intersections to insure all include ADA compliant curb ramps, pedestrian signal heads and push buttons, pedestrian scale lighting, and high visibility crosswalks.**
- **Implement roundabouts at recommended intersections. See Appendix C for concepts.**
- **Develop ongoing education and enforcement programs to increase motorist, pedestrian, and bicyclist awareness of the rules of the road.**

16. RESOURCES

Funding Sources

- FHWA Highway Safety Improvement Program: <https://www.nj.gov/transportation/about/safety/hsip.shtm>
- NJTPA Local Safety Program & High Risk Rural Roads Program: <https://www.njtpa.org/local-safety>
- NJDOT Municipal Aid: <https://www.state.nj.us/transportation/business/localaid/municipaid.shtm>
- NJDOT County Aid: <https://www.state.nj.us/transportation/business/localaid/countyaid.shtm>
- Safe Routes to School: <https://www.state.nj.us/transportation/business/localaid/srts.shtm>
- Safe Streets to Transit: <https://www.state.nj.us/transportation/business/localaid/safe.shtm>
- Transportation Enhancements/Transportation Alternatives Program: <https://www.state.nj.us/transportation/business/localaid/enhancements.shtm>
- Sustainable Jersey: <http://www.sustainablejersey.com/grants-resources/>
- New Jersey Healthy Communities Network: <http://www.njhcn.org/>
- New Jersey Transportation Bank: <https://www.state.nj.us/transportation/>
- NJDEP Recreational Trails Grant: https://www.nj.gov/dep/grantandloanprograms/nhr_nrtp.htm

Design Guidance and Informational Guides

- New Jersey Complete Streets Design Guide: <http://njbikeped.org/wp-content/uploads/2017/05/Complete-Streets-Design-Guide.pdf>
- Passaic County Complete Streets Design Guidelines: <http://pomptonlakes-nj.gov/DocumentCenter/View/466/Passaic-County-Complete-Streets-Guidelines>
- New Jersey Bicycle Regulations: <http://www.state.nj.us/transportation/commuter/bike/regulations.shtm>
- FHWA Roundabout Informational Guide: http://www.virginiadot.org/business/resources/NCHRP_Report_672_Roundabout_Informational_Guide_2nd_Edition2010.pdf
- National Association of City Transportation Officials: <https://nacto.org/>

Complete Streets

- NJDOT Complete Streets Policy:
<https://www.state.nj.us/transportation/eng/completestreets/resources.shtm>
- NJ Bicycle & Pedestrian Resource Center: Complete Streets in NJ: <http://njbikeped.org/complete-streets-2/>
- National Complete Streets Coalition: <https://smartgrowthamerica.org/program/national-complete-streets-coalition/>

Encouragement and Educational Programming

- NJ Safe Routes to School Resource Center: <http://www.saferoutesnj.org/>
- TransOptions Safe Routes to School Program: <http://www.transoptions.org/safe-routes-to-school>
- National Center for Safe Routes to School: <http://guide.saferoutesinfo.org/encouragement/>

APPENDIX A

POMPTON LAKES COMPLETE STREETS POLICY

**RESOLUTION ESTABLISHING AND ADOPTING A COMPLETE STREETS POLICY FOR
THE BOROUGH OF POMPTON LAKES**

WHEREAS, a Complete Street is defined as a means to provide safe access for all users by designing and operating a comprehensive, integrated, connected multi-modal network of transportation options; and

WHEREAS, the benefits of Complete Streets include improving safety for pedestrians, bicyclists, children, older citizens, non-drivers, and the mobility challenged, as well as, those that cannot afford a car or choose to live car free; providing connections to bicycling and walking trip generators such as, employment, education, residential, recreation, retail centers, and public facilities; promoting healthy lifestyles; creating more livable communities; reducing traffic congestion and reliance on carbon fuels thereby reducing greenhouse gas emissions; and saving money by incorporating sidewalks, bike lanes, safe crossings and transit amenities into the initial design of a project, thus sparing the expense of future retrofits; and

WHEREAS, the Pompton Lakes Mayor and Borough Council wishes to establish a Complete Streets Policy through the planning, design, construction, maintenance and operation of new and retrofitted transportation facilities, enabling safe access and mobility; and

NOW, THEREFORE, be it resolved that the Mayor and Borough Council adopts the following Complete Streets Policy with the following goals and objectives:

1. Create a comprehensive, integrated, connected multi-modal network by facilitating connections to bicycling and walking trip generators such as employment, education, residential, recreational, and public facilities, as well as retail and transit centers.
2. Provide safe and accessible accommodations for existing and future pedestrian, bicycle and transit facilities.
3. Establish a checklist of pedestrian, bicycle and transit accommodations such as accessible sidewalks, curb ramps, crosswalks, countdown pedestrian signals, signs, curb extensions, pedestrian scale lighting, bike lanes, and shoulders for consideration in each project even if county or state jurisdiction applies. Additionally, checklist items shall take into consideration scenic and historic assets that may be impacted by any project as well as any possible "Green Streets" applications that can mitigate the impacts of flooding throughout the Borough;
4. Establishment of a procedure to evaluate resurfacing projects for Complete Streets inclusion according to length of project, local support, environmental constraints, right-of-way limitations, funding, and bicycle and/or pedestrian compatibility.
5. Transportation facilities constructed for long-term use shall anticipate likely future demand for bicycling and walking facilities and not preclude the provision of future improvements.
6. Designs shall address the need for bicyclists and pedestrians to cross corridors, as well as travel along them in a safe, accessible and convenient manner; therefore, the design of intersections, interchanges and bridges shall anticipate use by bicyclists and pedestrians.

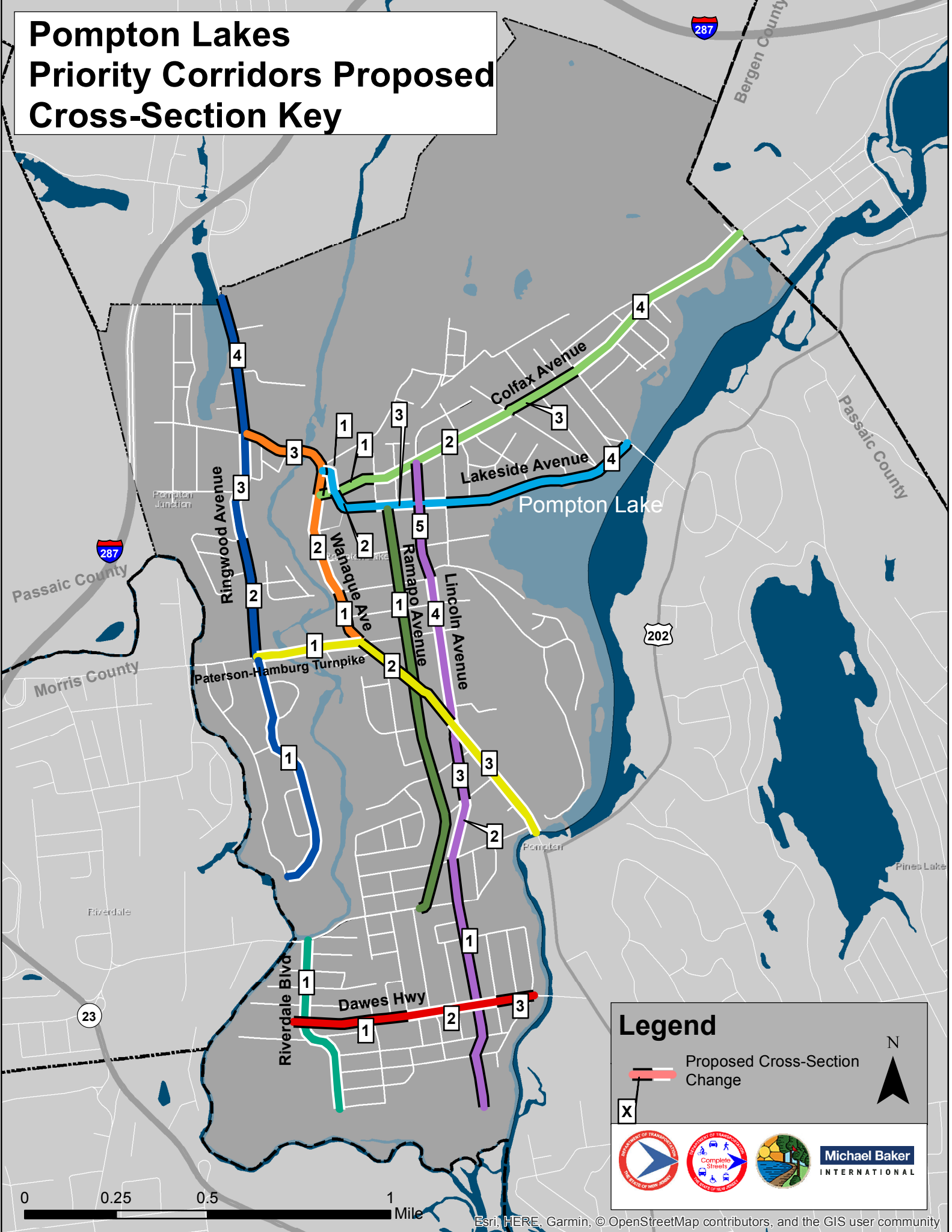
7. Bicycle and pedestrian facilities shall be designed and constructed to the best currently available standards and practices including the Passaic County Complete Streets Guidelines, the New Jersey Department of Transportation (NJDOT) Roadway Design Manual, the American Association of State Highway and Transportation Officials (AASHTO) Guide for the Development of Bicycle Facilities, the AASHTO Guide for the Planning, Design and Operation of Pedestrian Facilities, the National Association of City Transportation Officials (NACTO) Urban Street Design Guide, Manual of Uniform Traffic Control Devices and others as related.
8. Provisions shall be made for pedestrians and bicyclists when closing roads, bridges or sidewalks for construction projects as outlined in NJDOT Policy #705 – Accommodating Pedestrian and Bicycle Traffic During Construction.
9. Improvements shall also consider connections for Safe Routes to School, Safe Streets to Transit, Transit Villages, trail crossings and areas or population groups with limited transportation options;
10. Improvements shall comply with Title VII Environmental Justice, Americans with Disabilities Act (ADA) and complement the context of the surrounding community.
11. Exemptions to the Complete Streets Policy shall be presented to the Mayor and Borough Council in writing and documented with supporting data that indicates the reason for the decision and are limited to the following:
 - a) Non-motorized users are prohibited on the roadway
 - b) Scarcity of population, travel and attractors, both existing and future, indicate an absence of need for such accommodations.
 - c) Detrimental environmental or social impacts outweigh the need for these accommodations.
 - d) Cost of accommodations is excessively disproportionate to cost of the project.
 - e) The safety or timing of a project is compromised by the inclusion of Complete Streets.

BE IT FURTHER RESOLVED, that a certified copy of the Resolution shall be sent to the Passaic County Board of Chosen Freeholders and all Departments and Agencies having a responsibility for or connection with projects connected with the Pompton Lakes Complete Streets Policy.

APPENDIX B

BICYCLE FACILITY PROPOSED CROSS SECTIONS AND MAP

Pompton Lakes Priority Corridors Proposed Cross-Section Key



Legend

Proposed Cross-Section Change

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DEPARTMENT OF TRANSPORTATION THE STATE OF NEW JERSEY

DEPARTMENT OF TRANSPORTATION Complete Streets STATE OF NEW JERSEY

Michael Baker INTERNATIONAL



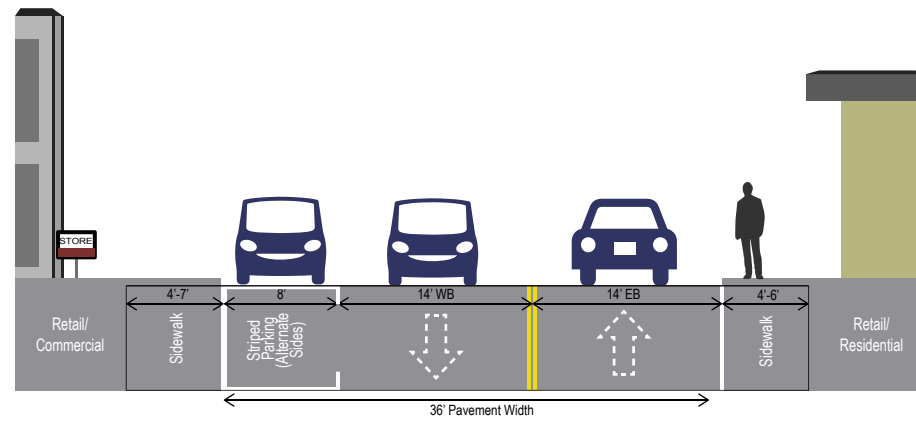
Colfax Avenue West

Jurisdiction: County
Speed Limit: 25 MPH

Traffic Volume (2012): 5,347
On-Street Parking: Yes

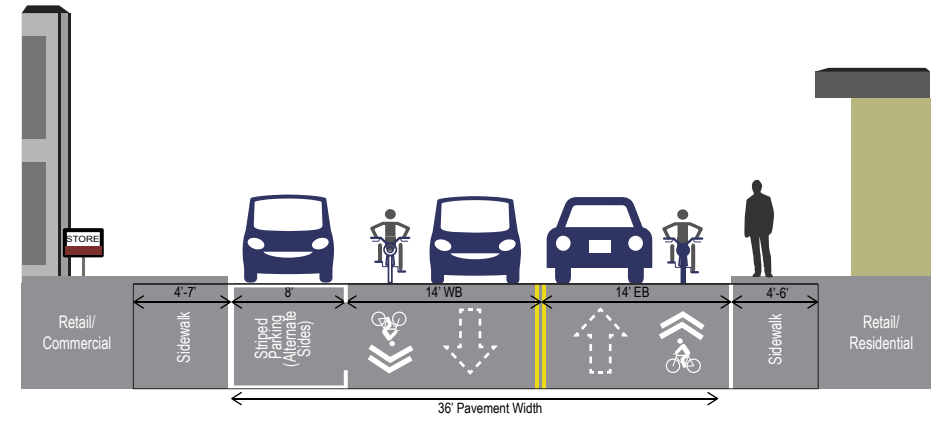
Existing Conditions

Section 1: Wanaque Avenue to Adrian Street
Length: 0.16 Mile

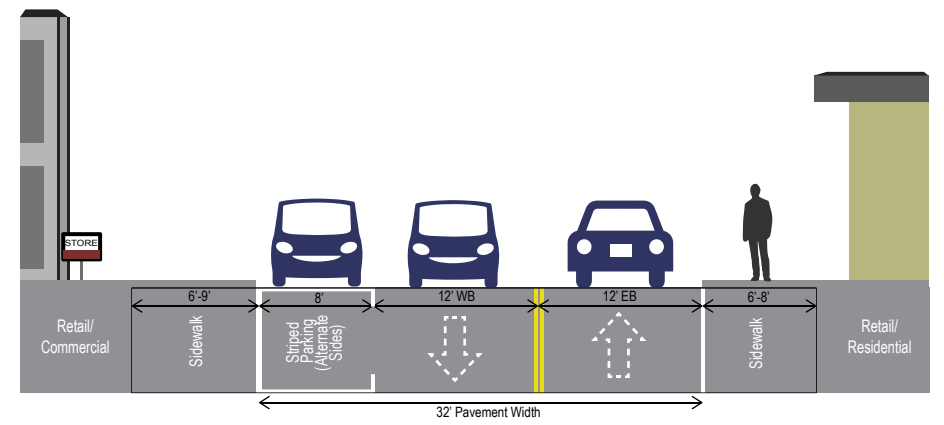


Proposed Improvements

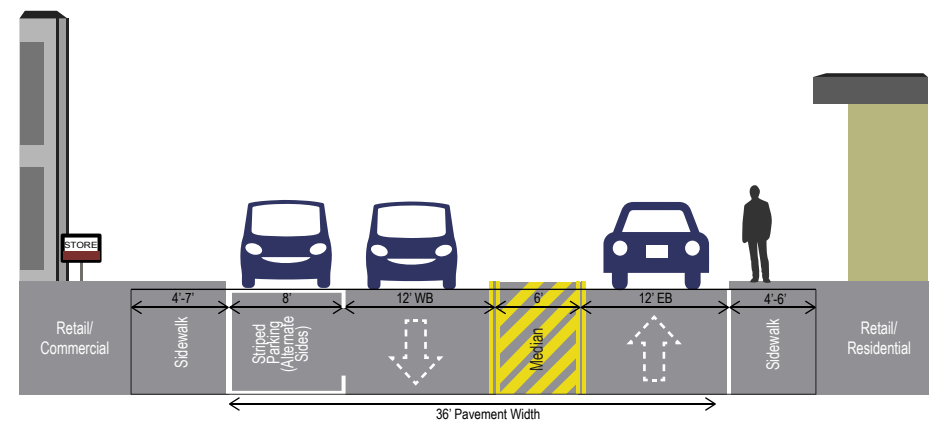
Wanaque Avenue to Adrian Street - Alternative A



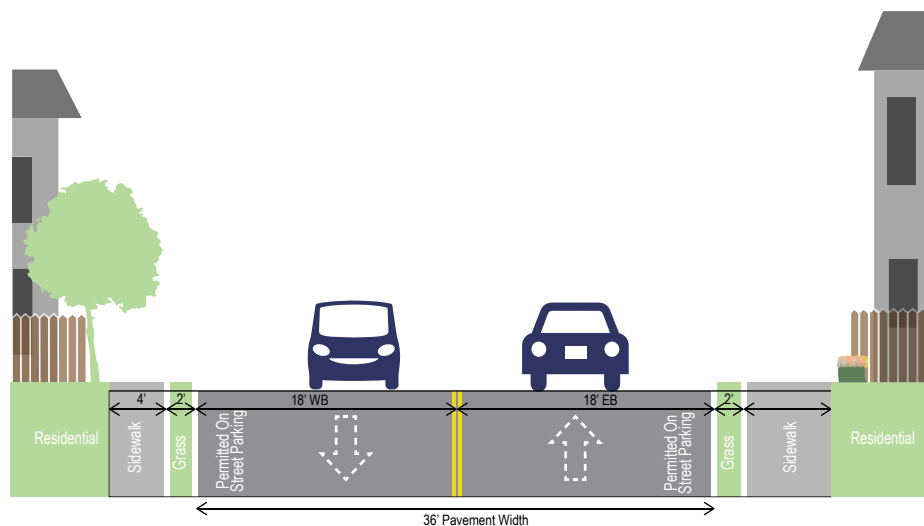
Wanaque Avenue to Adrian Street - Alternative B



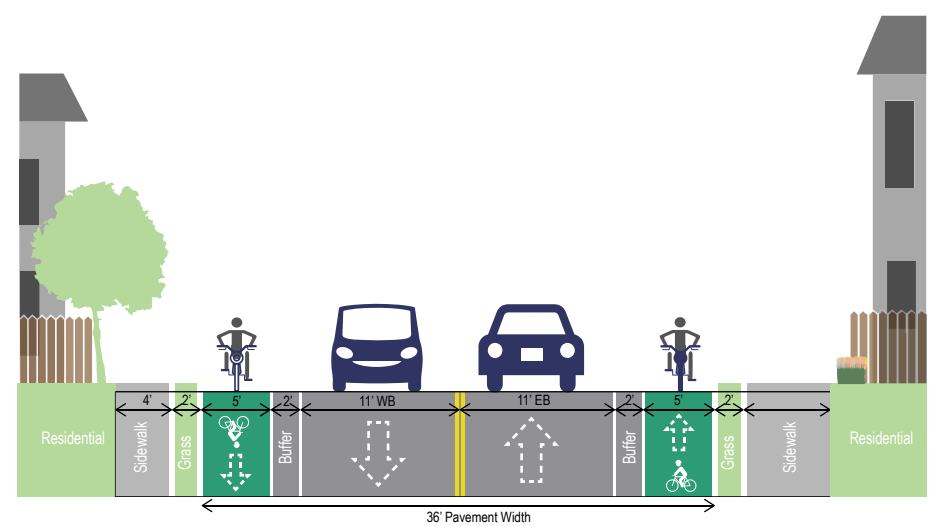
Wanaque Avenue to Adrian Street - Alternative C



Section 2: Adrian Street to Grant Avenue
Length: 0.42 Mile



Adrian Street to Grant Avenue - Alternative



Colfax Avenue East

Jurisdiction: County/Municipal
Speed Limit: 25 MPH

Traffic Volume (2012): 5,347
On-Street Parking: Yes

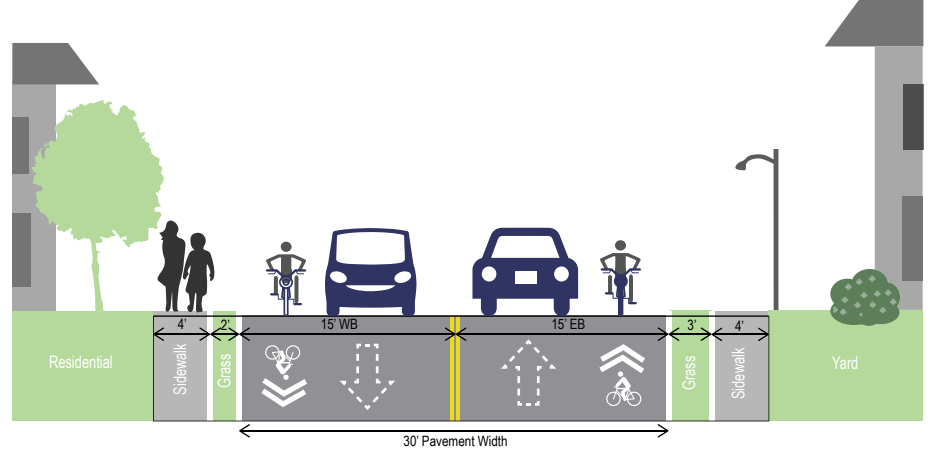
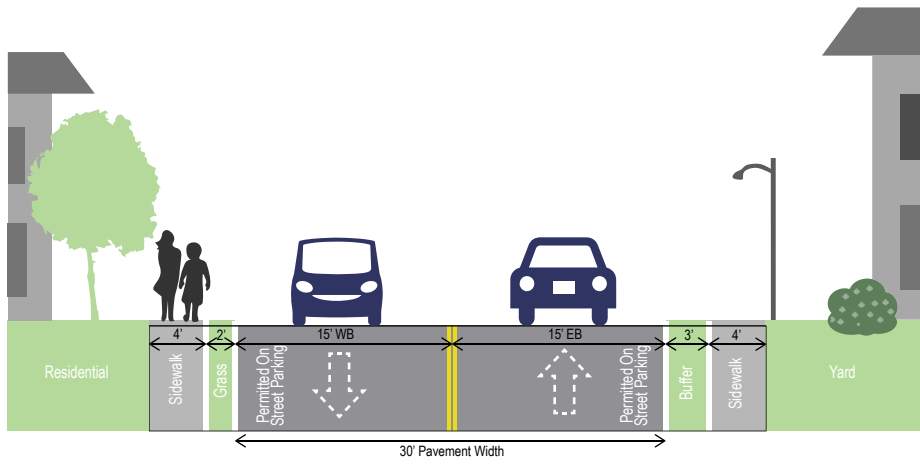
Existing Conditions

Proposed Improvements

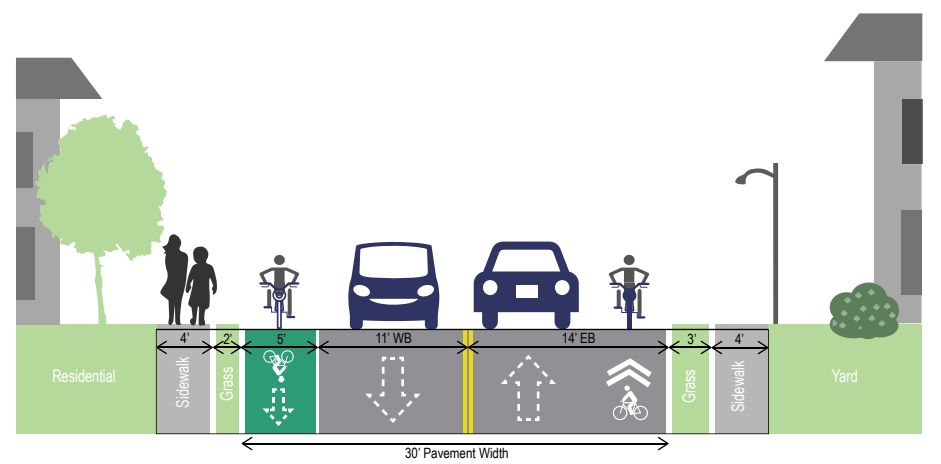
Section 3: Grant Avenue to Perrin Avenue

Length: .21 Mile

Grant Avenue to Perrin Avenue - Alternative A



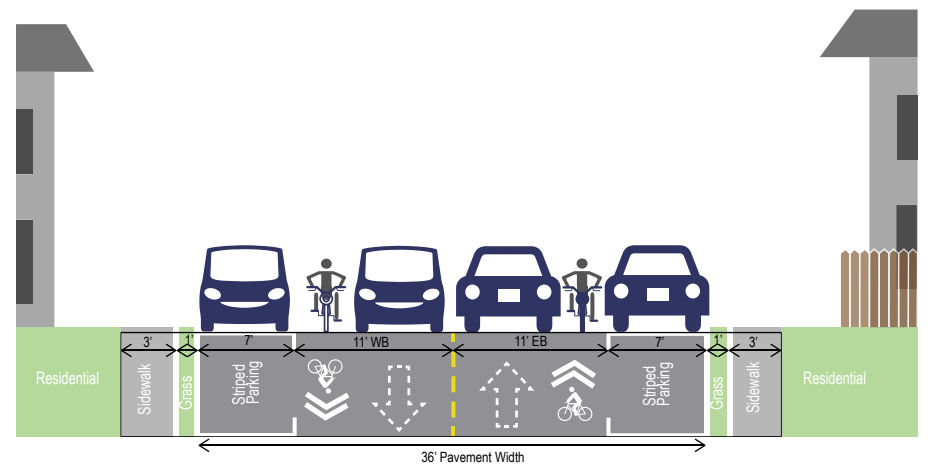
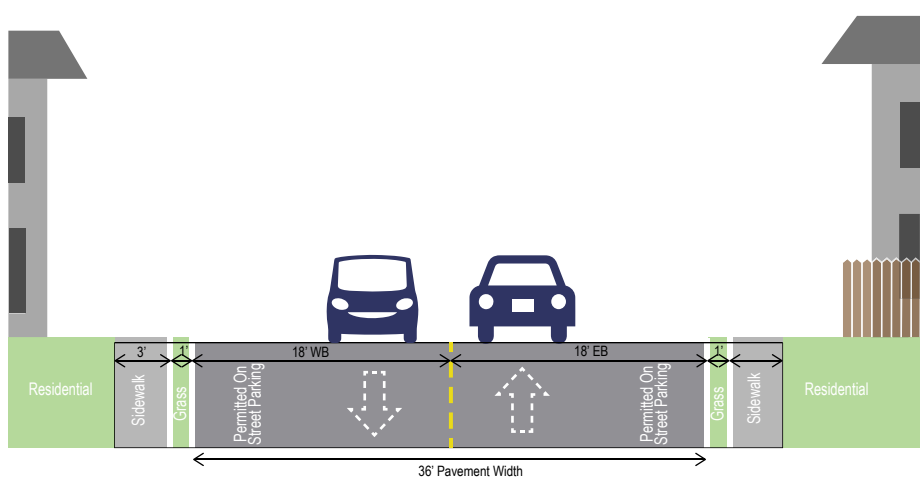
Grant Avenue to Perrin Avenue - Alternative B



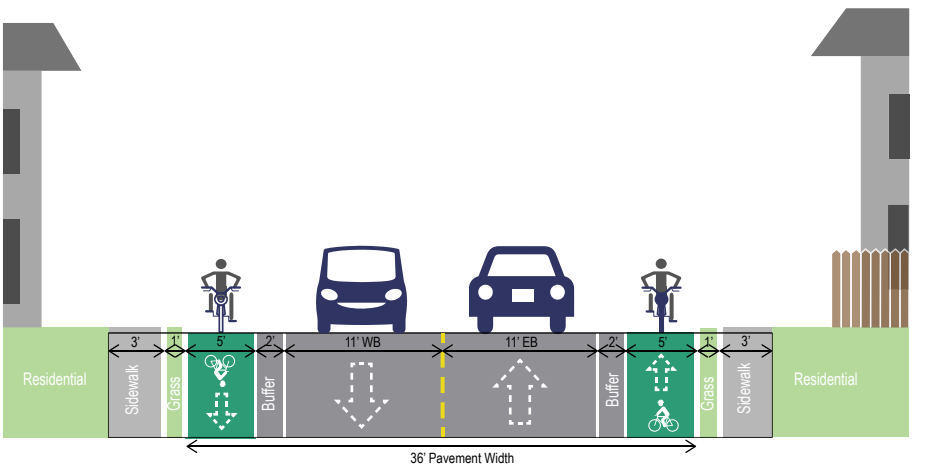
Section 4: Perrin Avenue to Pierson Miller Drive

Length: .58 Mile

Perrin Avenue to Pierson Miller Drive - Alternative A



Perrin Avenue to Pierson Miller Drive - Alternative B



Dawes Highway

Jurisdiction: Municipal
Speed Limit: 25 MPH

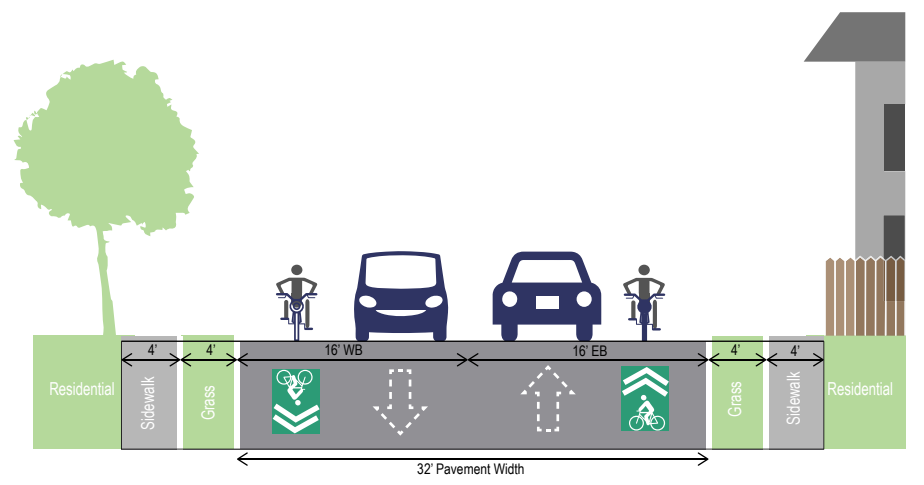
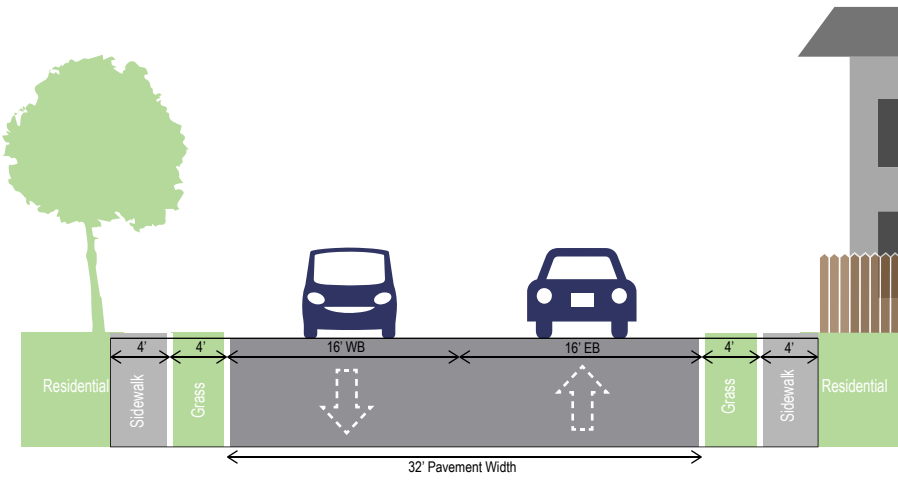
Traffic Volume: Unknown
On-Street Parking: No

Existing Conditions

Proposed Improvements

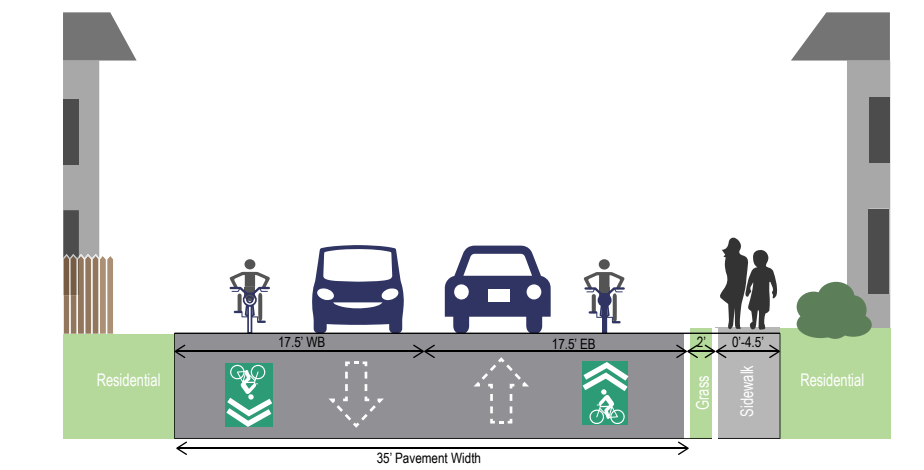
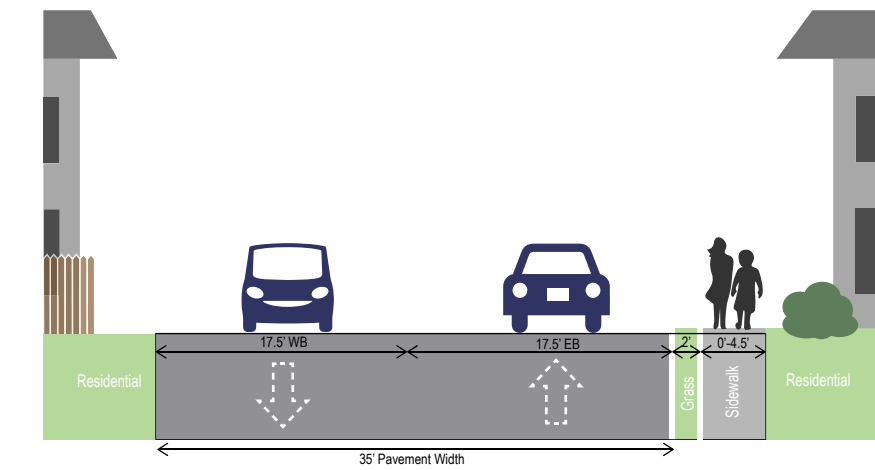
Section 1: Riverdale Boulevard to Olive Avenue
Length: 0.28 Mile

Riverdale Boulevard to Olive Avenue - Alternative



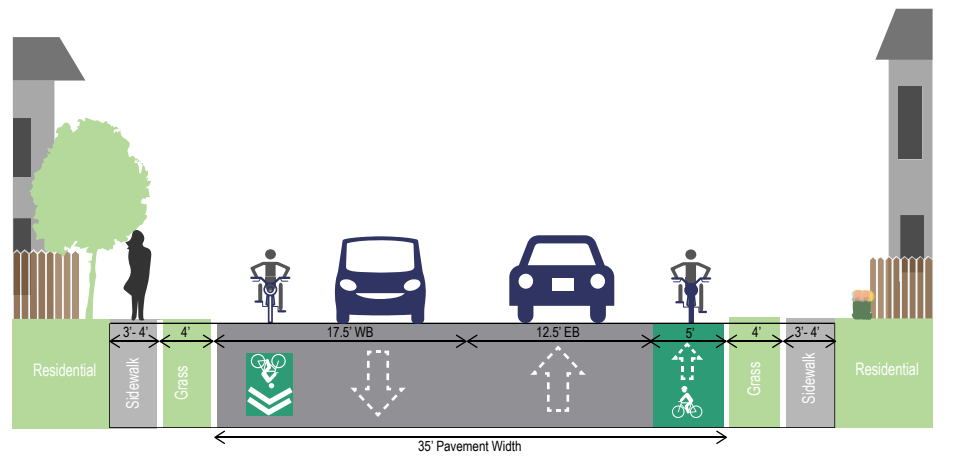
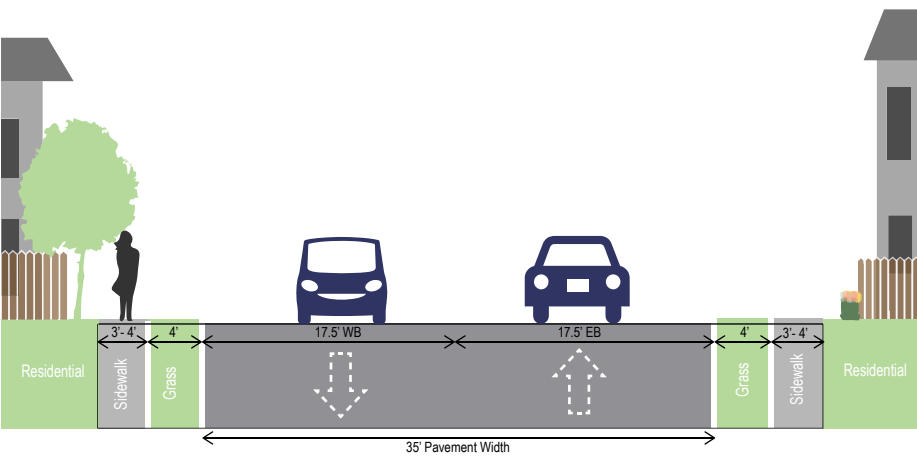
Section 2: Olive Avenue to Washington Avenue
Length: 0.28 Mile

Olive Avenue to Washington Avenue - Alternative



Section 3: Washington Avenue to Riveredge Drive
Length: 0.11 Mile

Washington Avenue to Riveredge Drive - Alternative



Paterson-Hamburg Turnpike

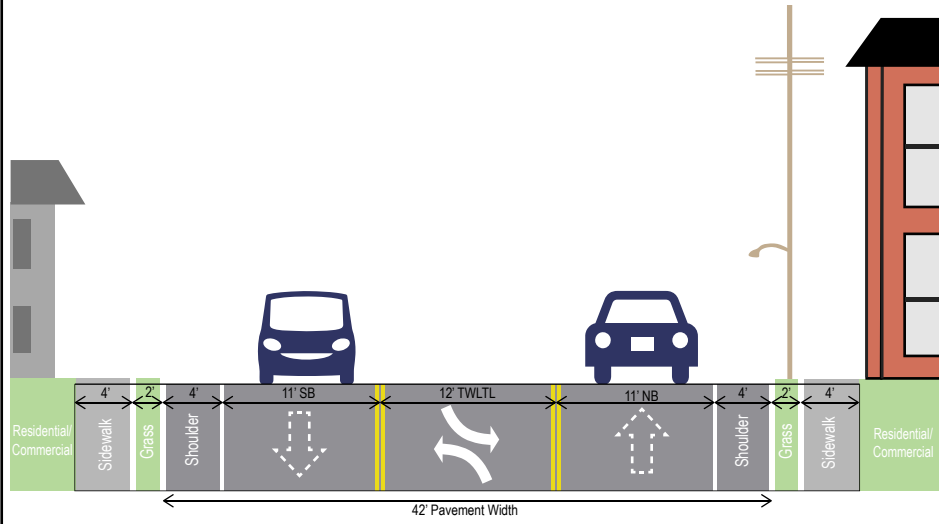
Jurisdiction: County
Speed Limit: 45 MPH

Traffic Volume (2012): 19,921
On-Street Parking: No

Existing Conditions

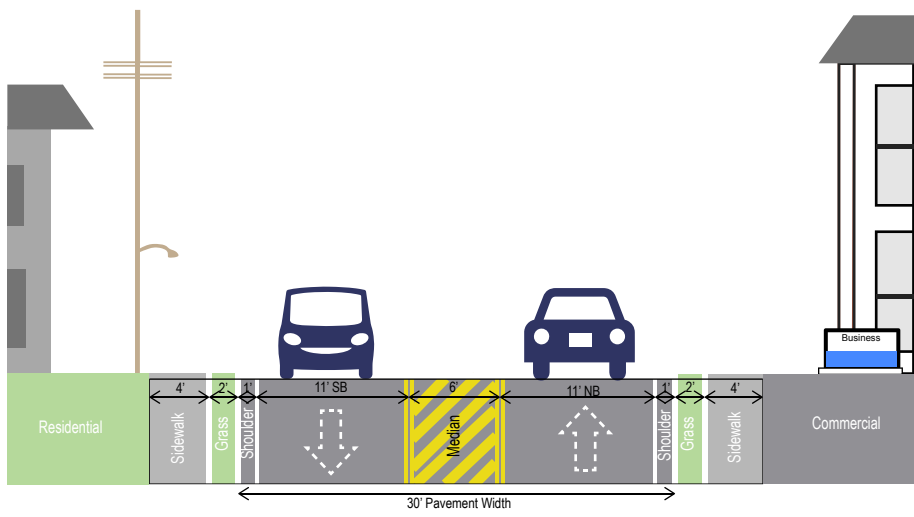
Proposed Improvements

Section 1: Riverview Avenue to Ramapo Avenue
Length: 0.51 Mile

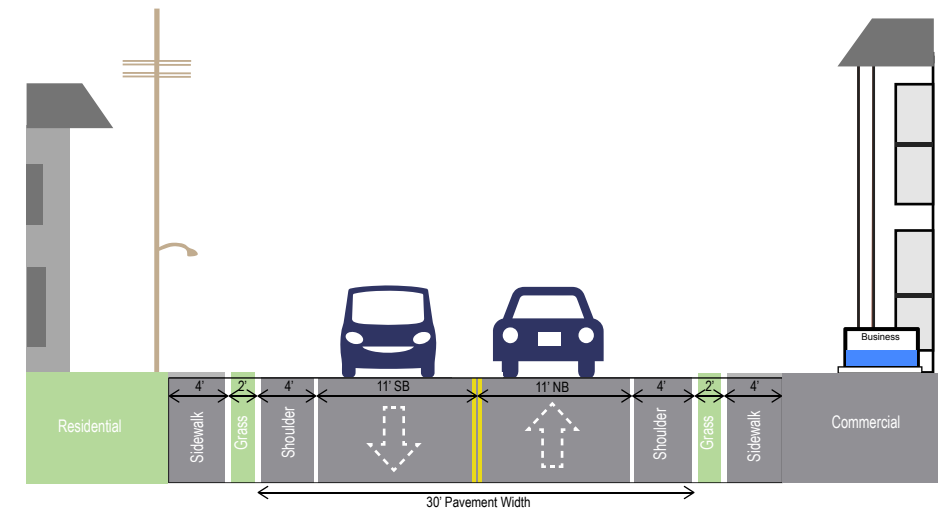


**Maintain Existing
Cross-Section**

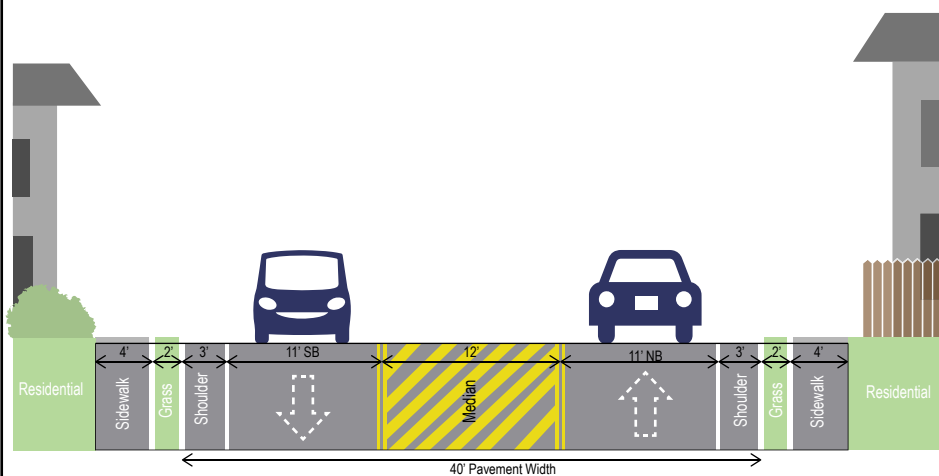
Section 2: Ramapo Avenue to Wanaque Avenue
Length: 0.22 Mile



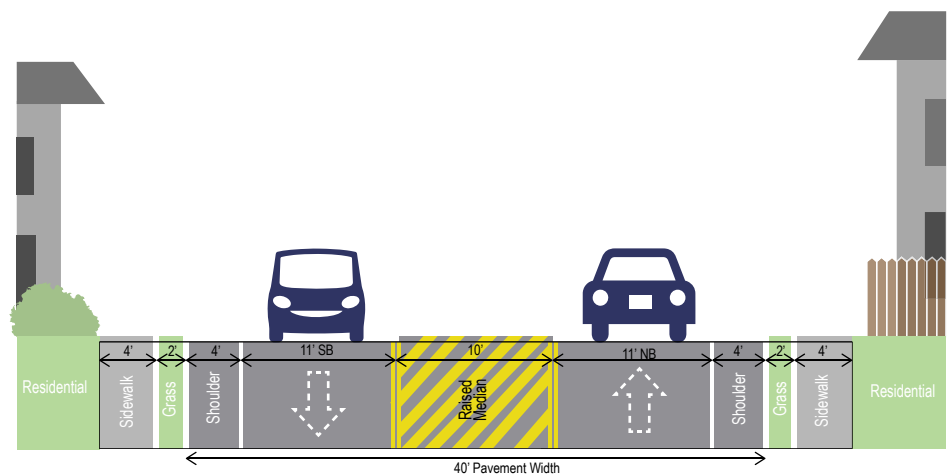
Ramapo Avenue to Wanaque Avenue - Alternative



Section 3: Wanaque Avenue to Ringwood Avenue
Length: 0.25 Mile

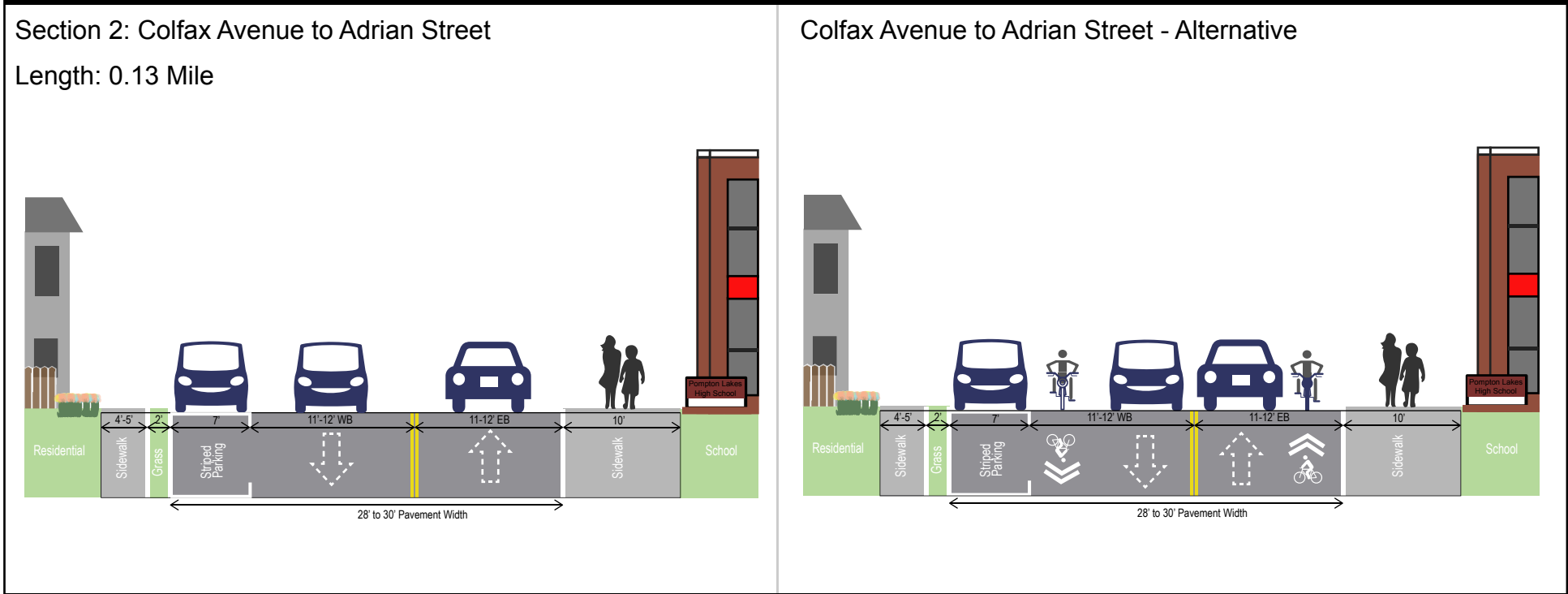
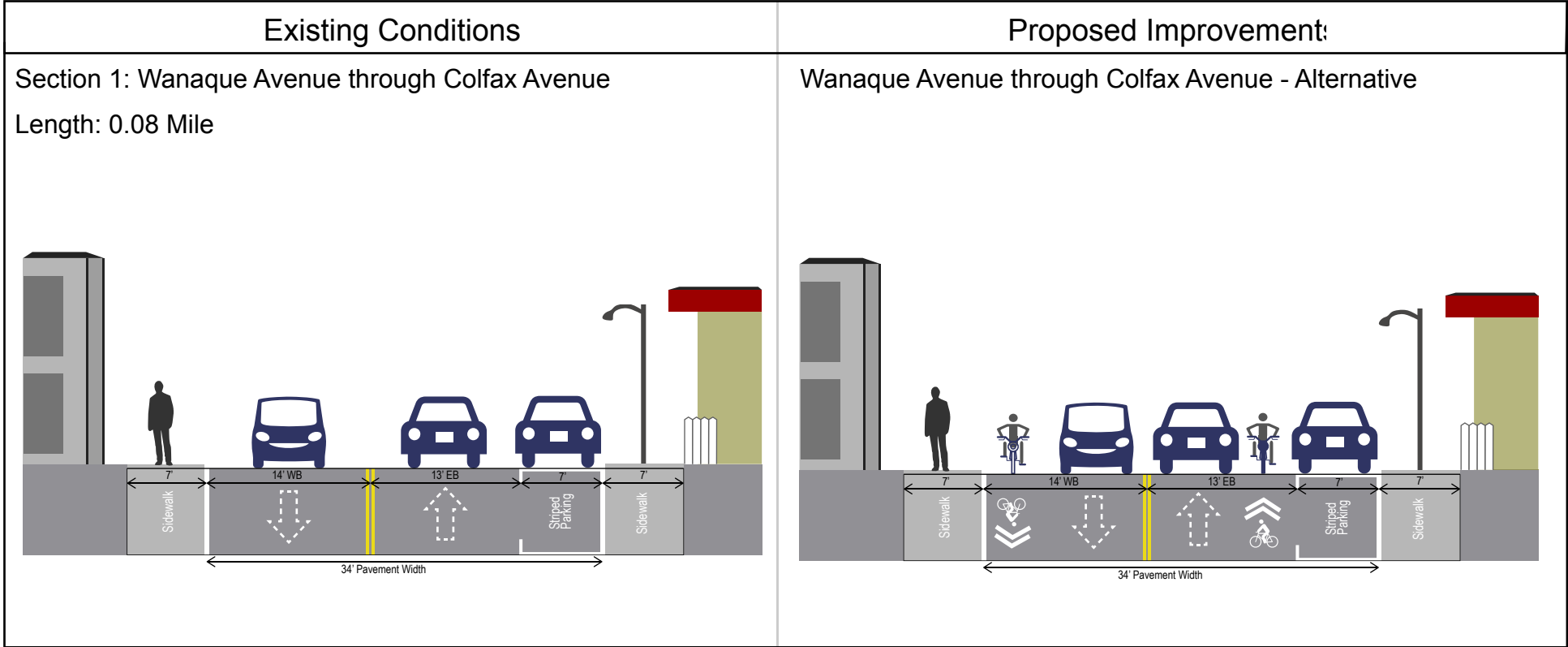


Wanaque Avenue to Ringwood Avenue - Alternative



Lakeside Avenue West

Jurisdiction: Municipal	Traffic Volume: Unknown
Speed Limit: 25 MPH	On-Street Parking: Yes



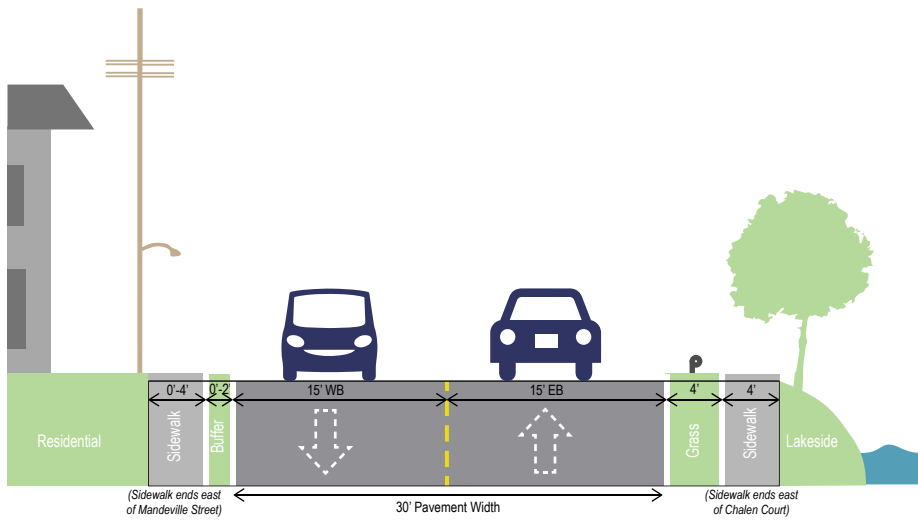
Lakeside Avenue East

Jurisdiction: County
 Speed Limit: 25 MPH

Traffic Volume (2012): 5,347
 On-Street Parking: No

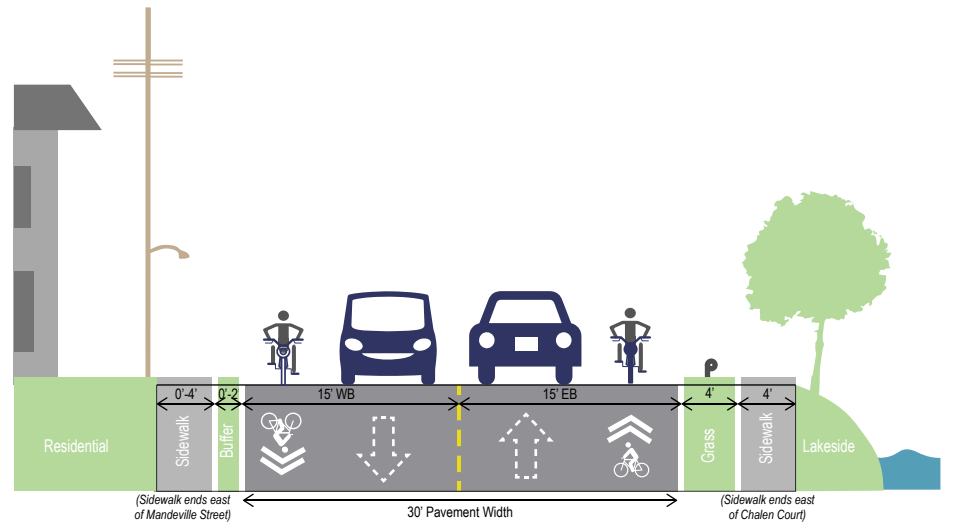
Existing Conditions

Section 4: Van Avenue to Jefferson Avenue
 Length: 0.54 Mile

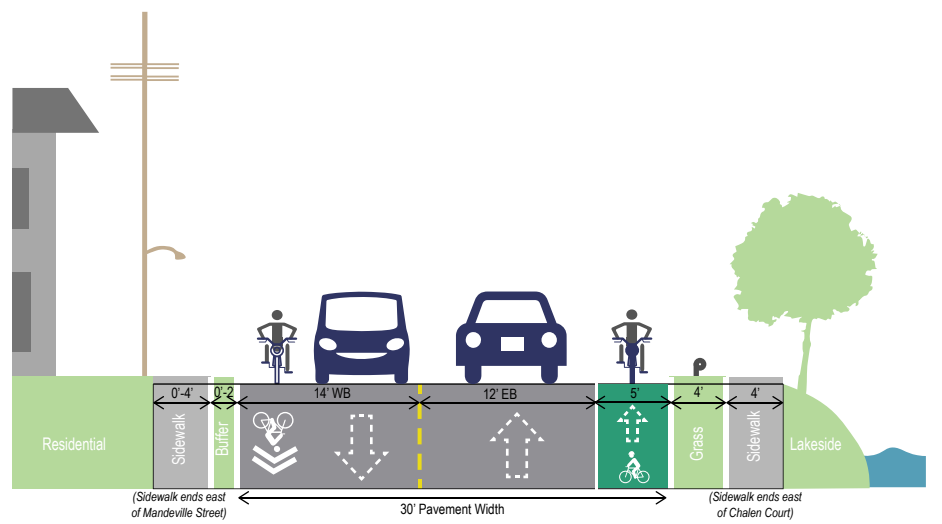


Proposed Improvements

Van Avenue to Jefferson Avenue - Alternative A



Van Avenue to Jefferson Avenue - Alternative B



Lincoln Avenue South

Jurisdiction: Municipal

Traffic Volume: Unknown

Speed Limit: 25 MPH

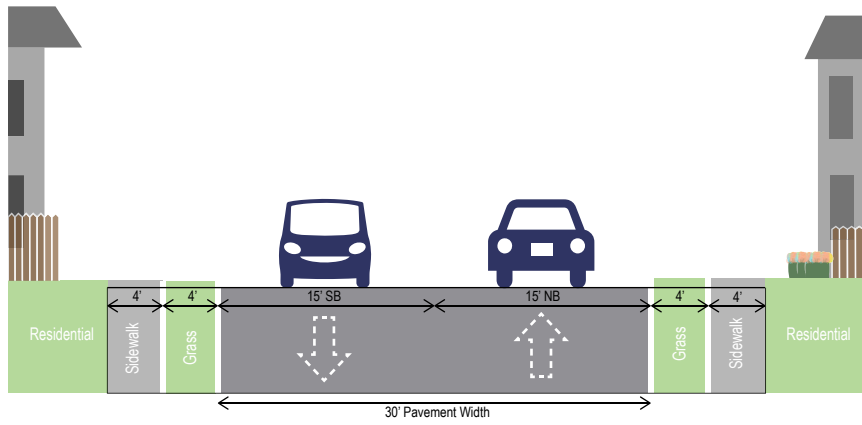
On-Street Parking: No (with the exception of the Elementary School)

Existing Conditions

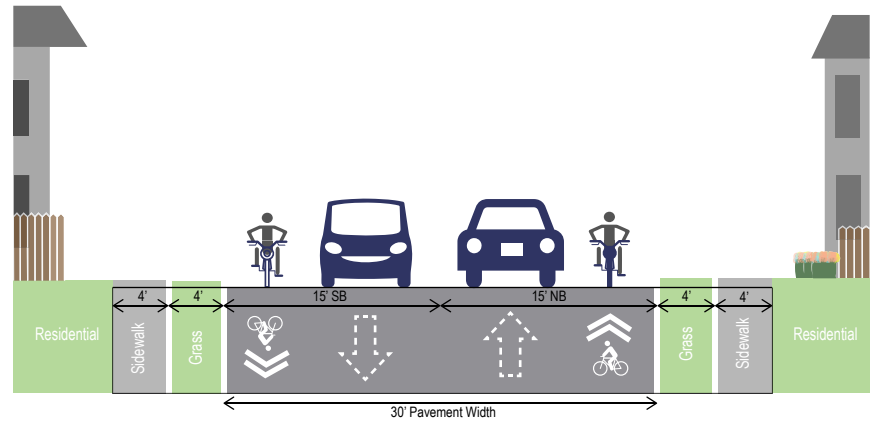
Proposed Improvements

Section 1: Poplar Ave to Riverview Road

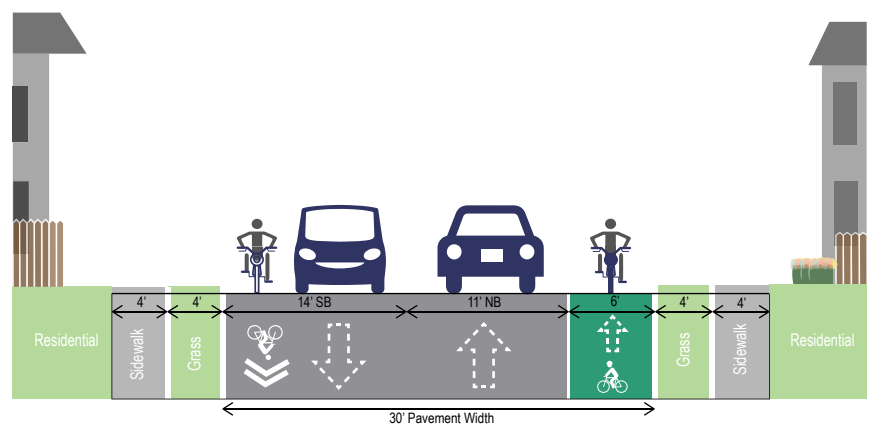
Length: 0.52 Mile



Poplar Ave to Riverview Road- Alternative A

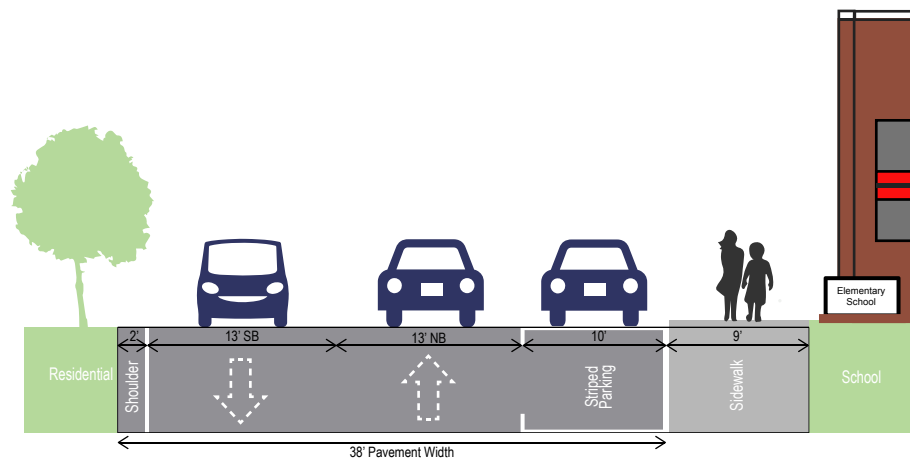


Poplar Ave through Riverview Road - Alternative B

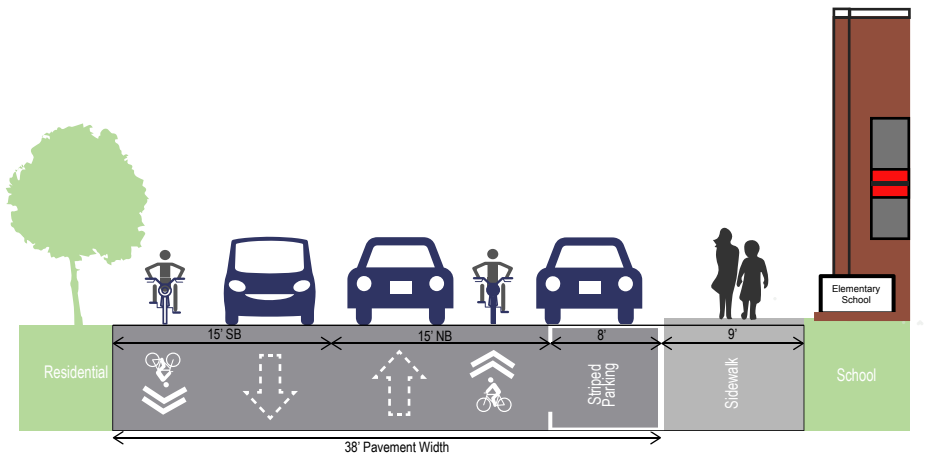


Section 2: Riverview Road to Pequannock Avenue (Due to Elementary School)

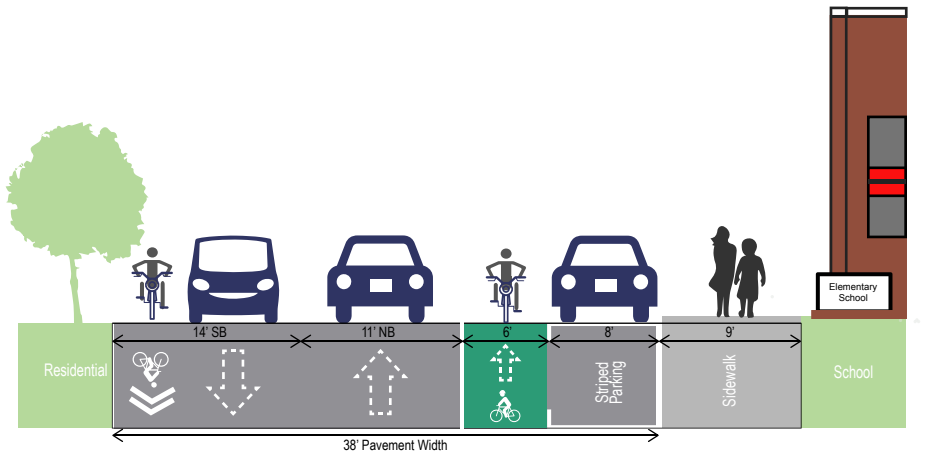
Length: 0.16 Mile



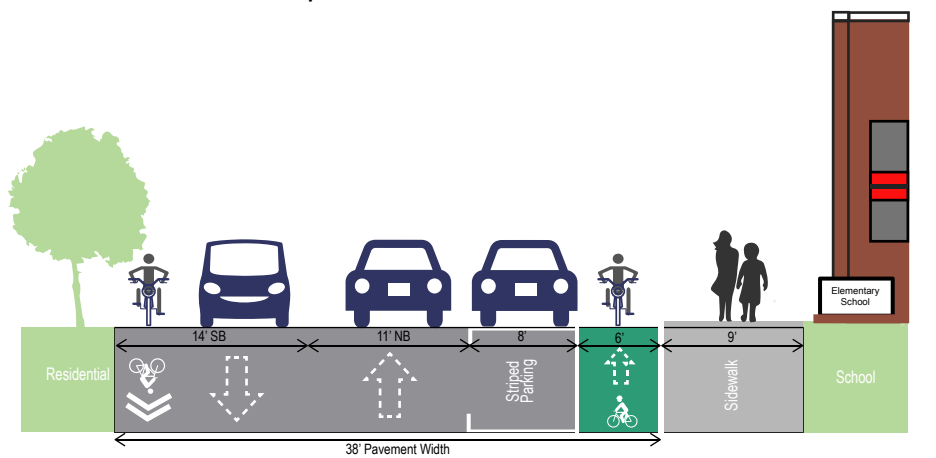
Riverview Road to Pequannock Avenue - Alternative A



Riverview Road to Pequannock Avenue - Alternative B



Riverview Road to Pequannock Avenue - Alternative C



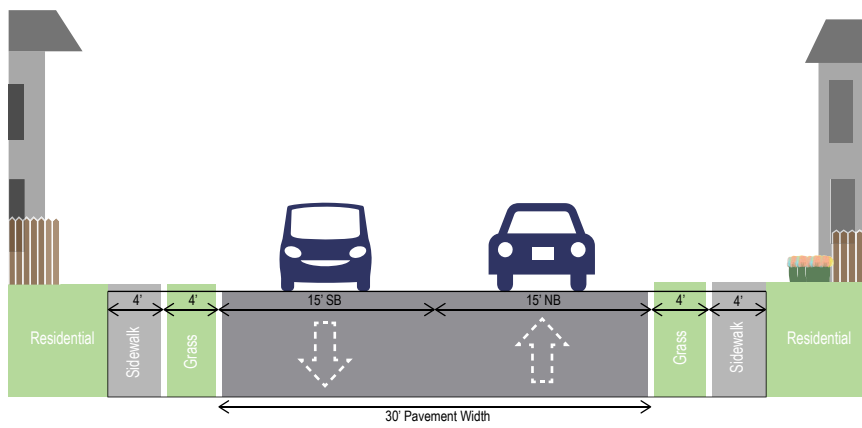
Lincoln Avenue Middle

Jurisdiction: Municipal
Speed Limit: 25 MPH

Traffic Volume: Unknown
On-Street Parking: No

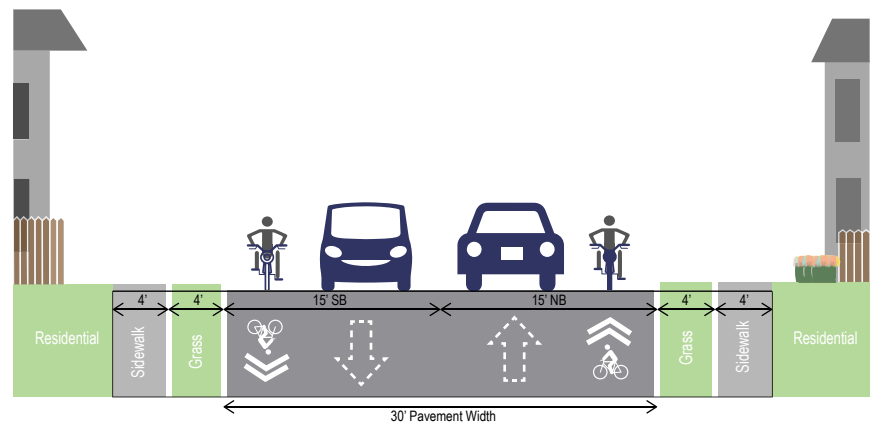
Existing Conditions

Section 3: Pequannock Avenue to Paterson-Hamburg Turnpike
Length: 0.19 Mile

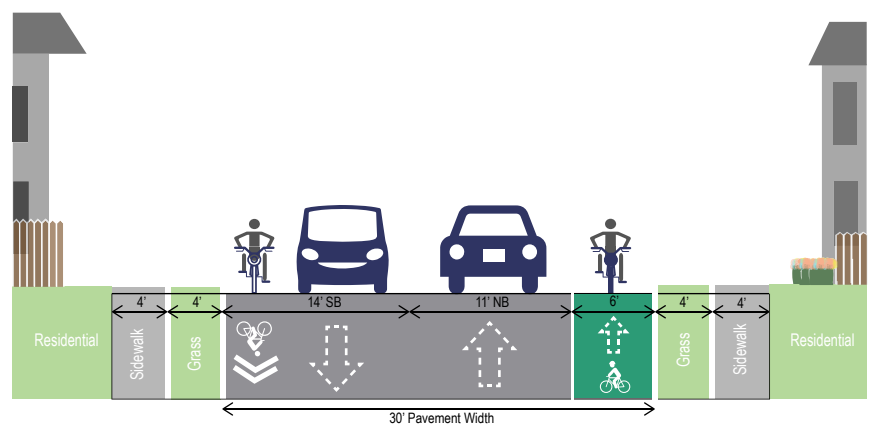


Proposed Improvements

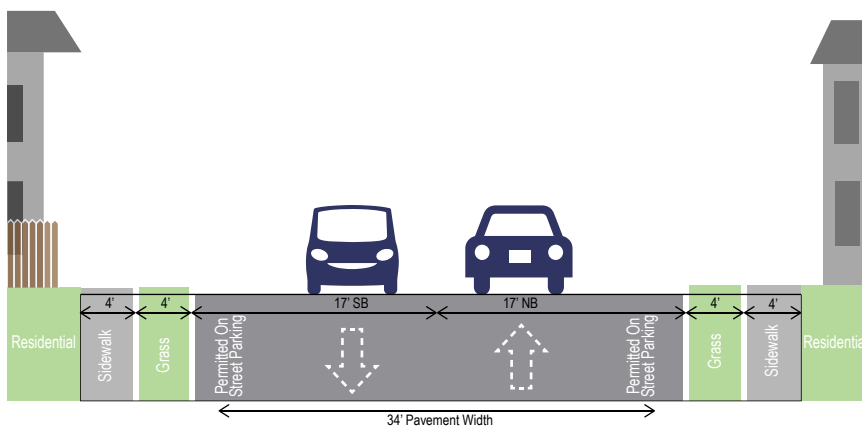
Pequannock Avenue to Paterson-Hamburg Turnpike - Alternative A



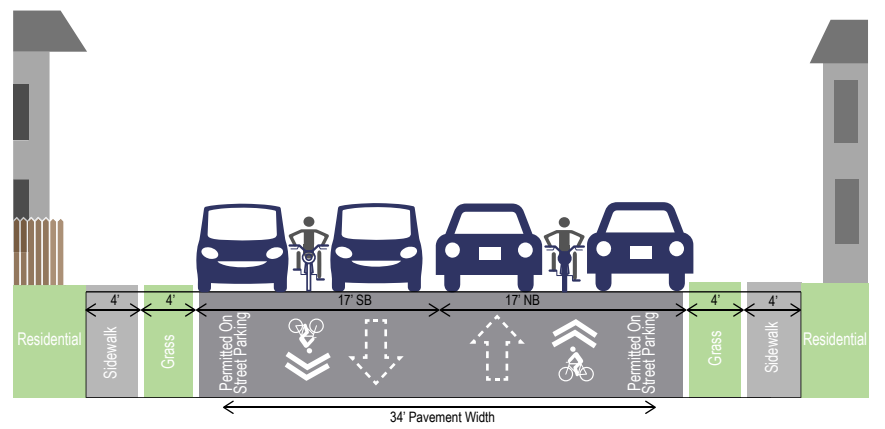
Pequannock Avenue to Paterson-Hamburg Turnpike - Alternative B



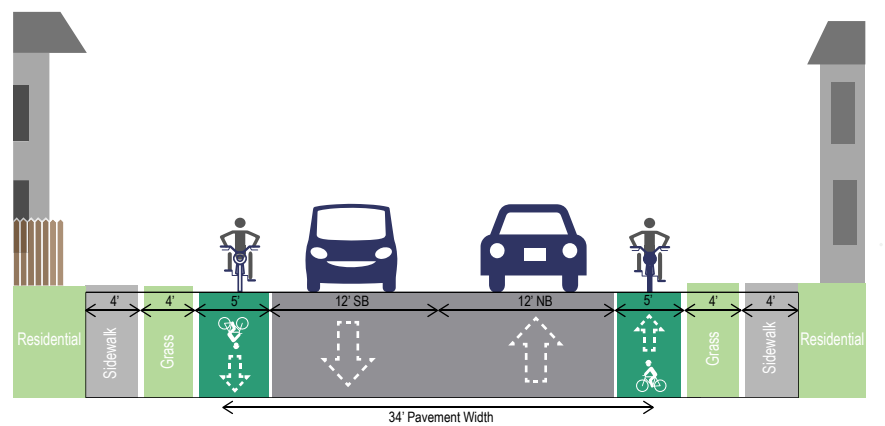
Section 4: Paterson-Hamburg Turnpike to Pompton Avenue
Length: 0.39 Mile



Paterson-Hamburg Turnpike to Pompton Avenue - Alternative A



Paterson-Hamburg Turnpike to Pompton Avenue - Alternative B



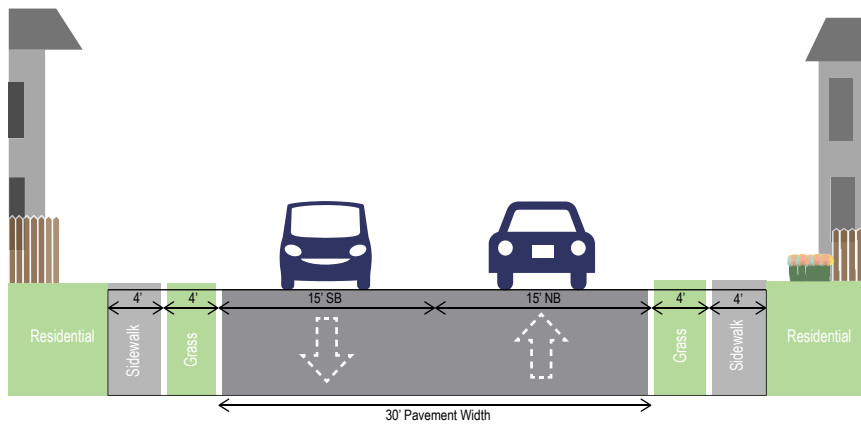
Lincoln Avenue North

Jurisdiction: Municipal
 Speed Limit: 25 MPH

Traffic Volume: Unknown
 On-Street Parking: No

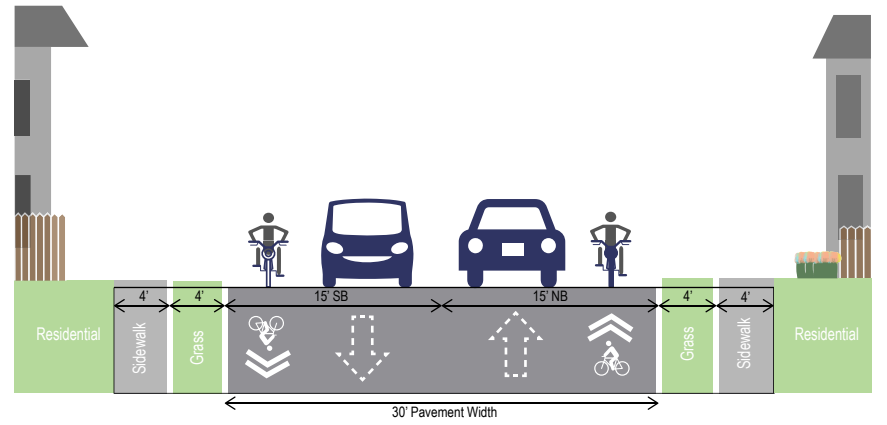
Existing Conditions

Section 5: Pompton Avenue to Colfax Avenue
 Length: 0.32 Mile

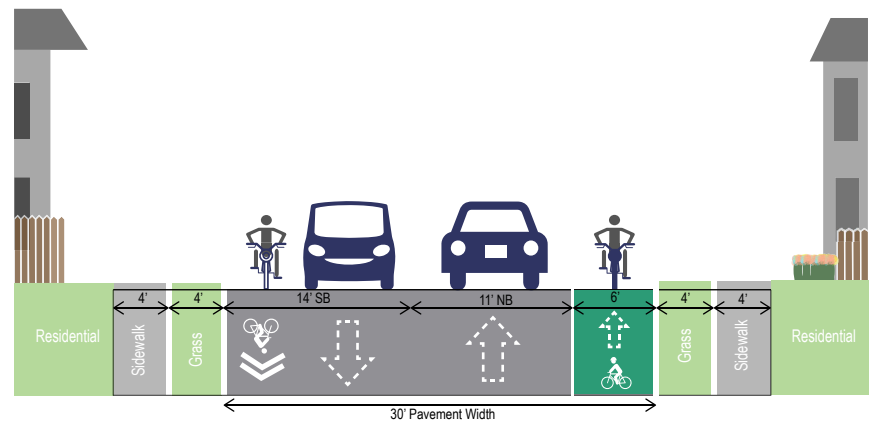


Proposed Improvements

Pompton Avenue to Colfax Avenue - Alternative A



Pompton Avenue to Colfax Avenue - Alternative B



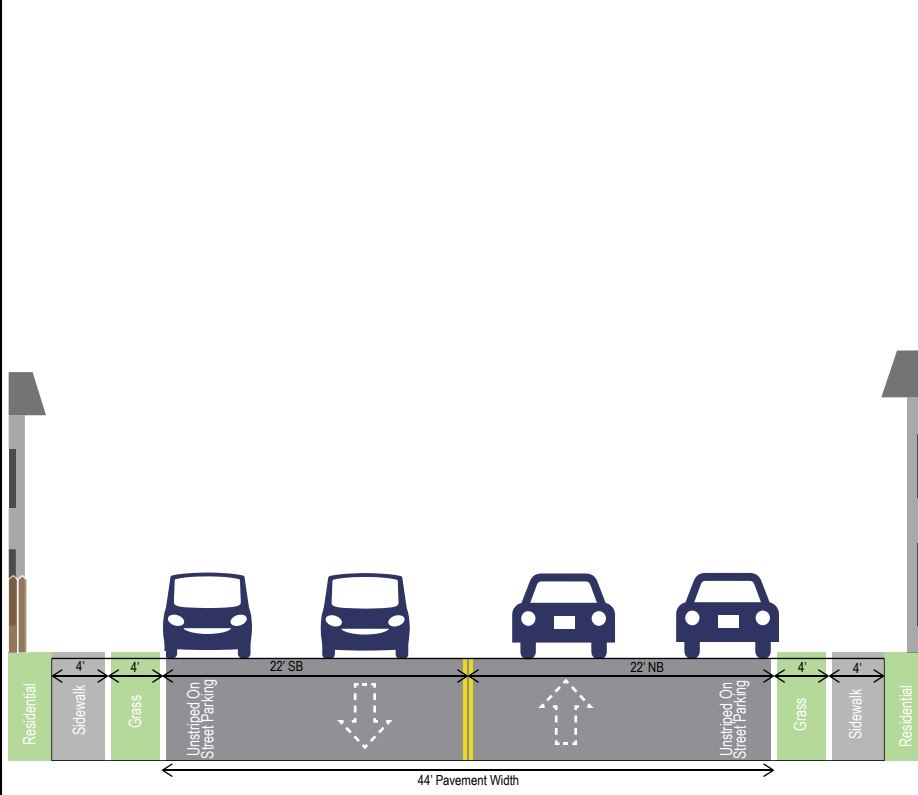
Ramapo Avenue

Jurisdiction: County
Speed Limit: 25 MPH

Traffic Volume (2012): 4,228
On-Street Parking: Yes

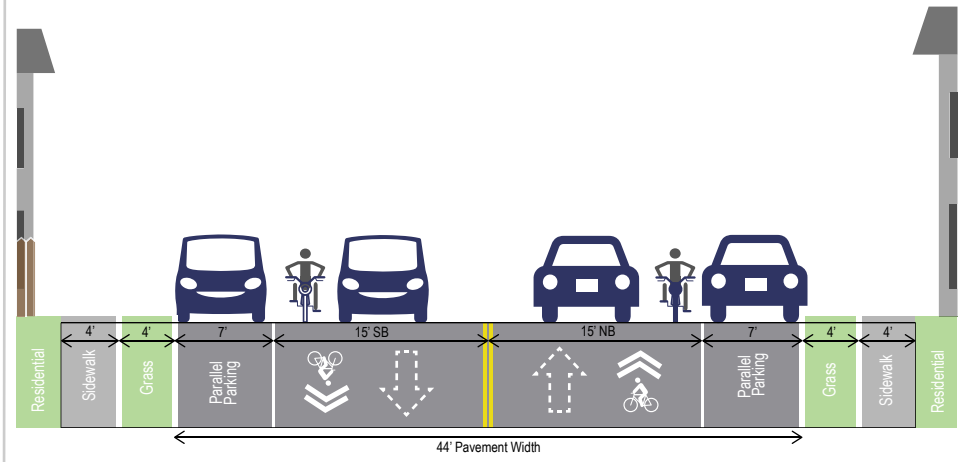
Existing Conditions

Section 1: Riverdale Road to Lakeside Avenue
Length: 1.12 Miles

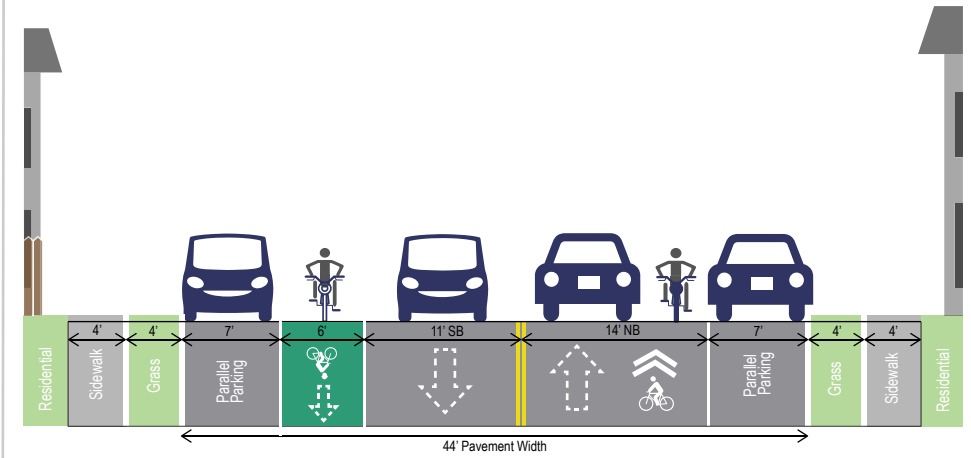


Proposed Improvements

Riverdale Road to Lakeside Avenue - Alternative A



Riverdale Road to Lakeside Avenue - Alternative B



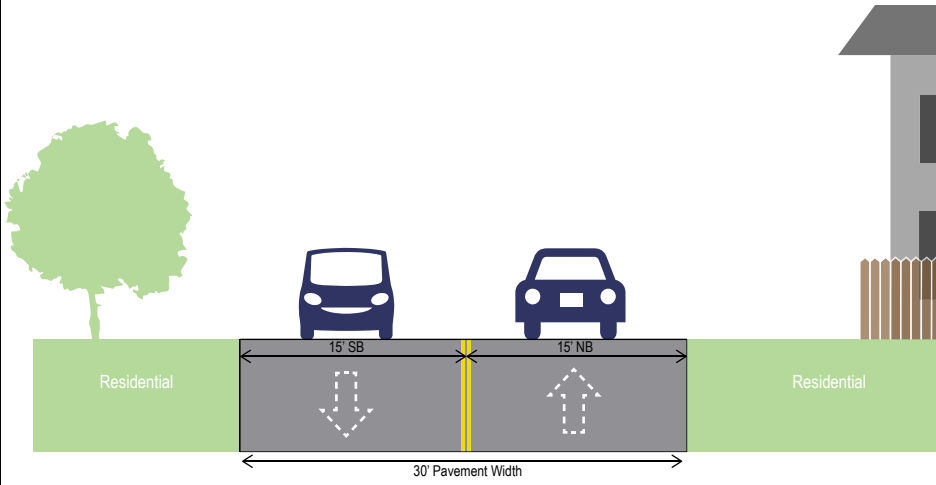
Ringwood Avenue South

Jurisdiction: County
Speed Limit: 35 MPH

Traffic Volume (2011): 7,088
On-Street Parking: No

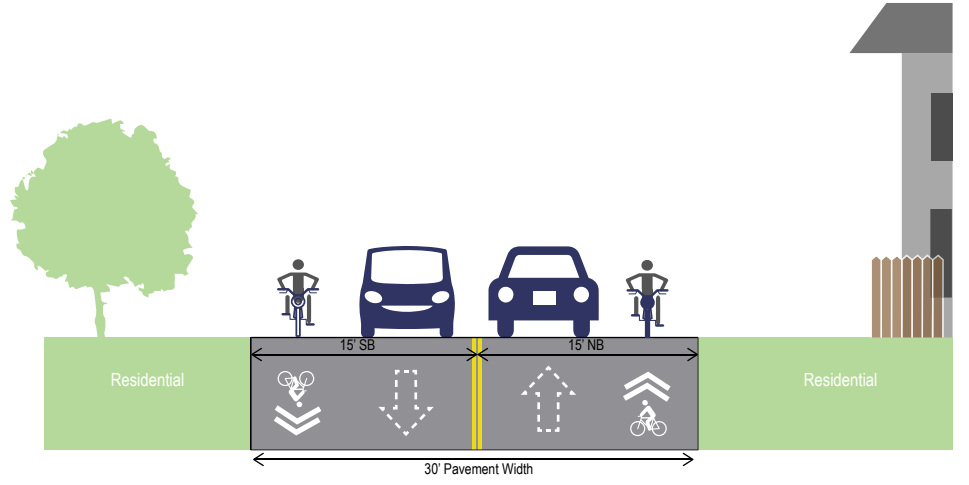
Existing Conditions

Section 1: Garden Road to Paterson-Hamburg Turnpike
Length: 0.68 Mile

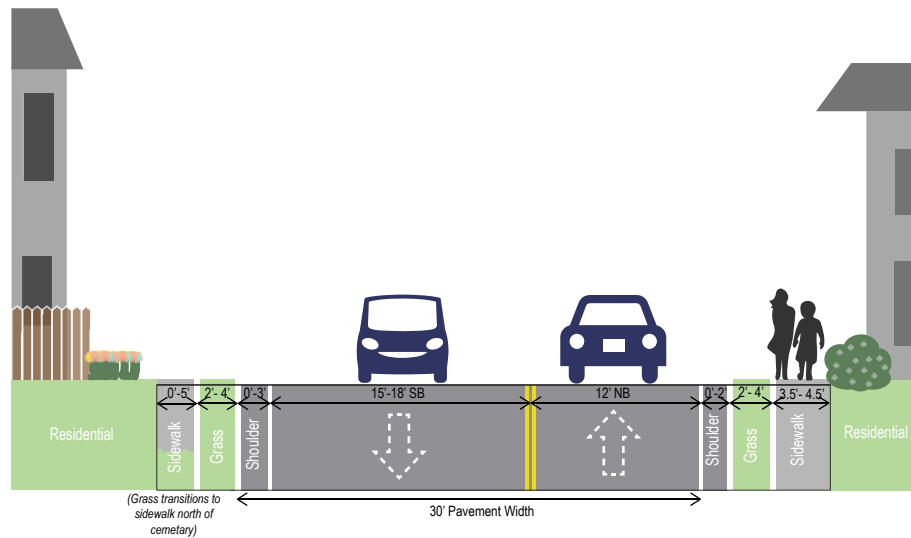


Proposed Improvements

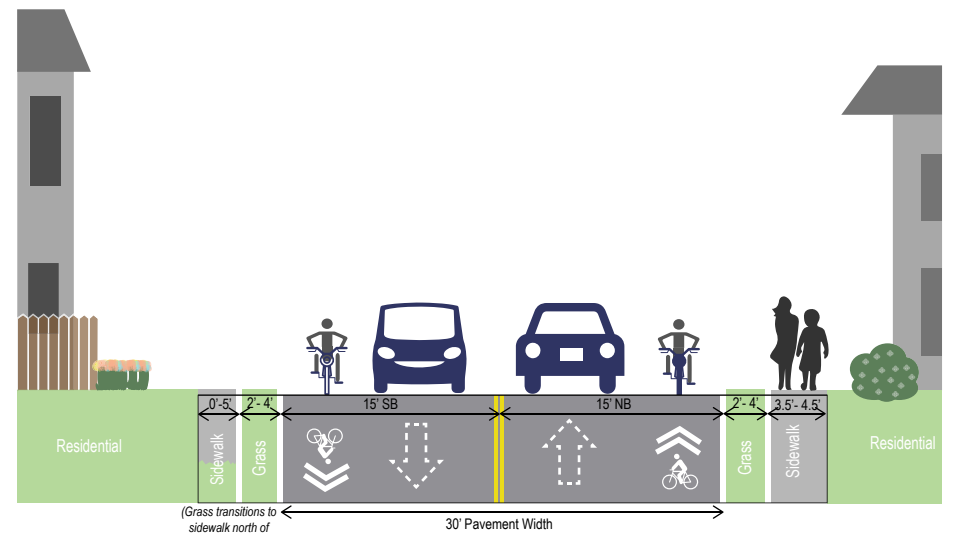
Garden Road to Paterson-Hamburg Turnpike - Alternative A



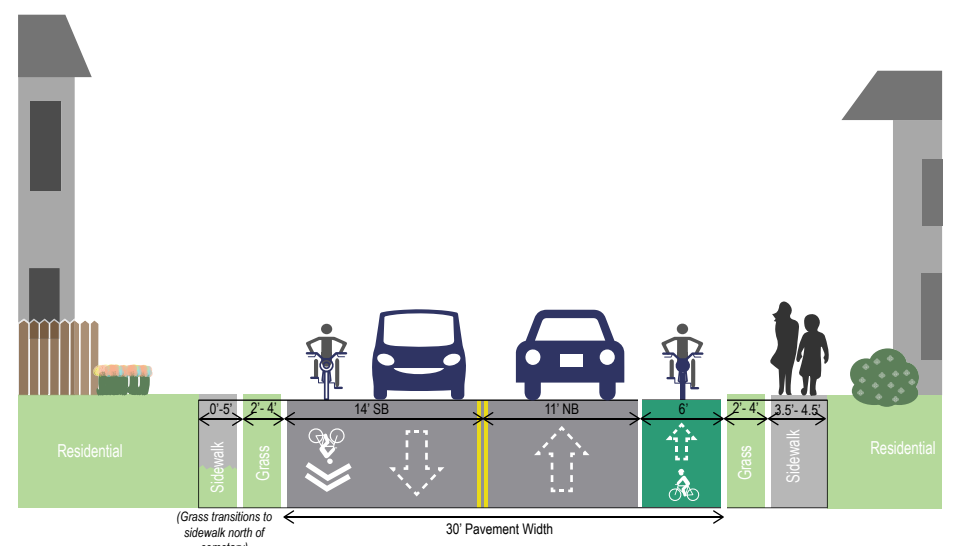
Section 2: Paterson-Hamburg Turnpike to Stiles Court
Length: 0.33 Mile



Paterson-Hamburg Turnpike to Stiles Court - Alternative A



Paterson-Hamburg Turnpike to Stiles Court - Alternative B



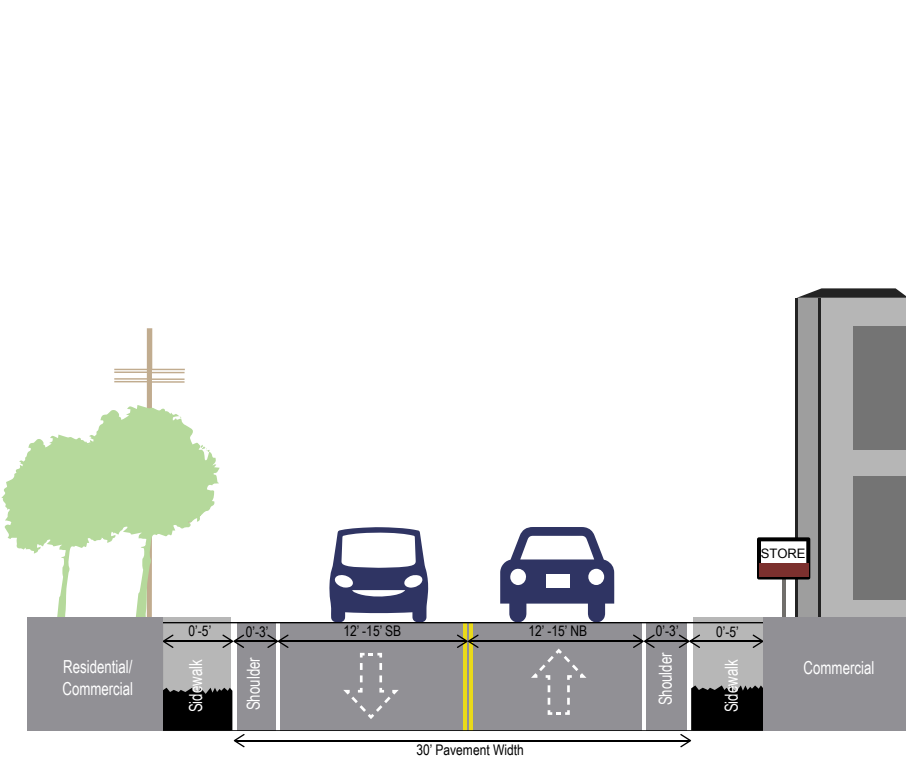
Ringwood Avenue North

Jurisdiction: County
Speed Limit: 35 MPH

Traffic Volume (2011): 7,088
On-Street Parking: No

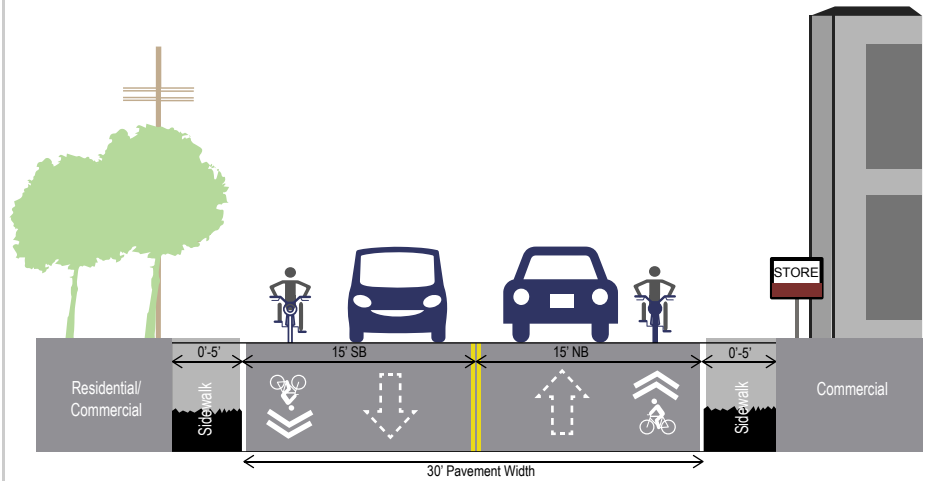
Existing Conditions

Section 3: Stiles Court to Willard Street/Wanaque Avenue
Length: 0.29 Mile

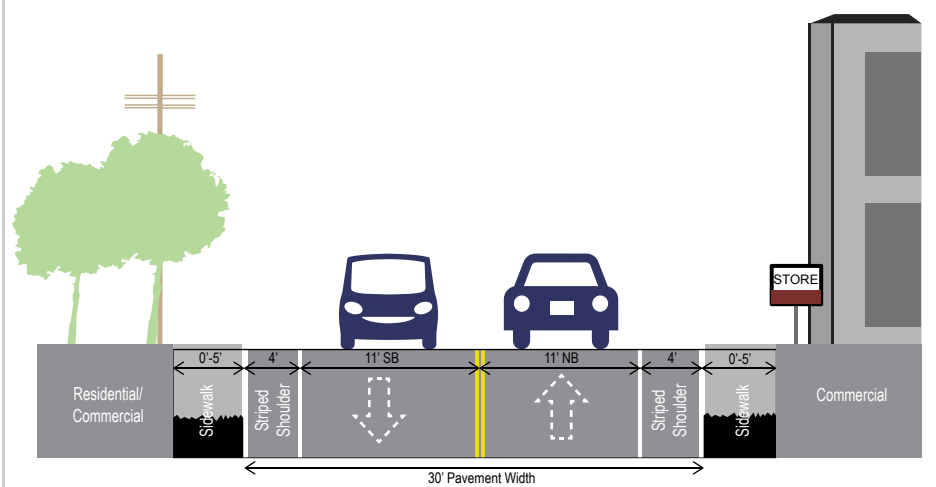


Proposed Improvements

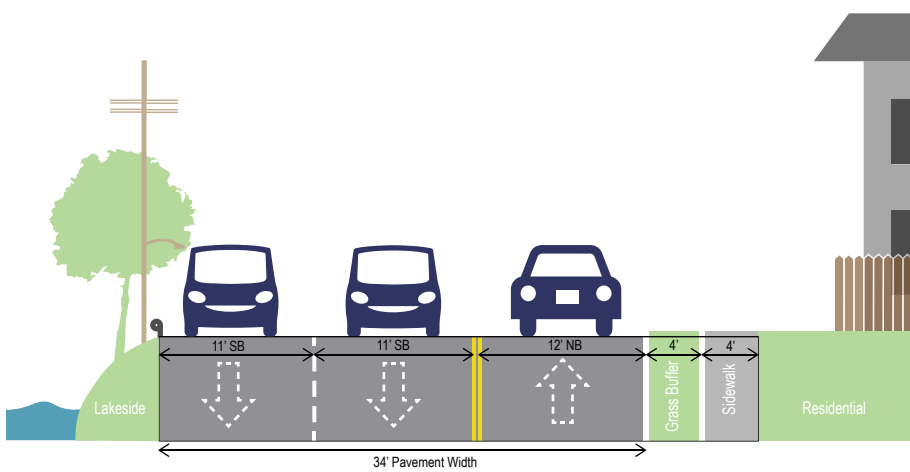
Stiles Court to Willard Street/Wanaque Avenue - Alternative A



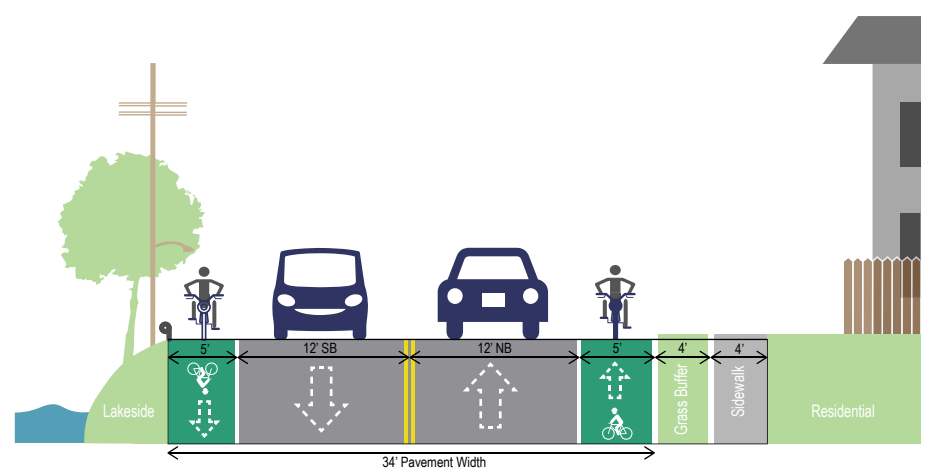
Stiles Court to Willard Street/Wanaque Avenue - Alternative B



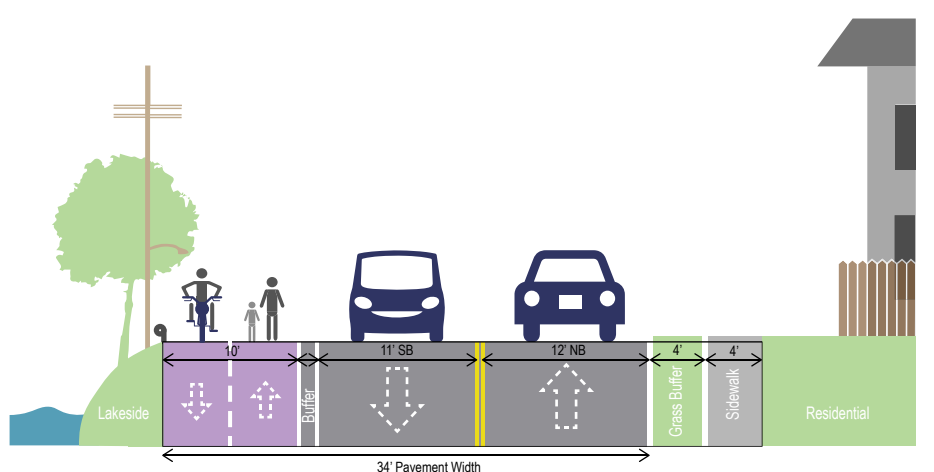
Section 4: Willard Street/Wanaque Avenue to Carr Street
Length: 0.29 Mile



Willard Street/Wanaque Avenue to Carr Street - Alternative A



Willard Street/Wanaque Avenue to Carr Street - Alternative B



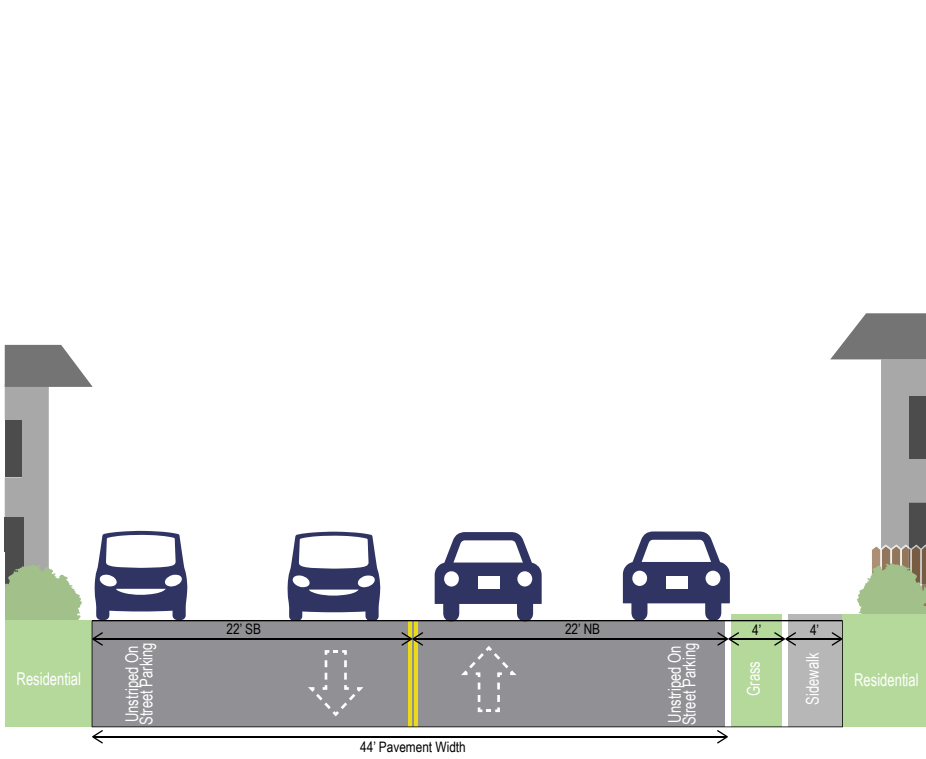
Riverdale Boulevard

Jurisdiction: Municipal
Speed Limit: 25 MPH

Traffic Volume: Unknown
On-Street Parking: Yes

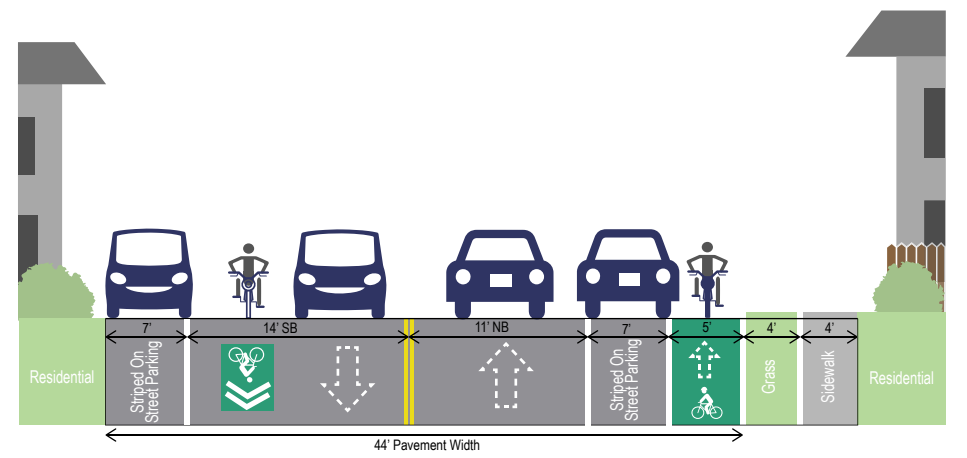
Existing Conditions

Section 1: Poplar Avenue through Riverdale Road
Length: 0.40 Mile

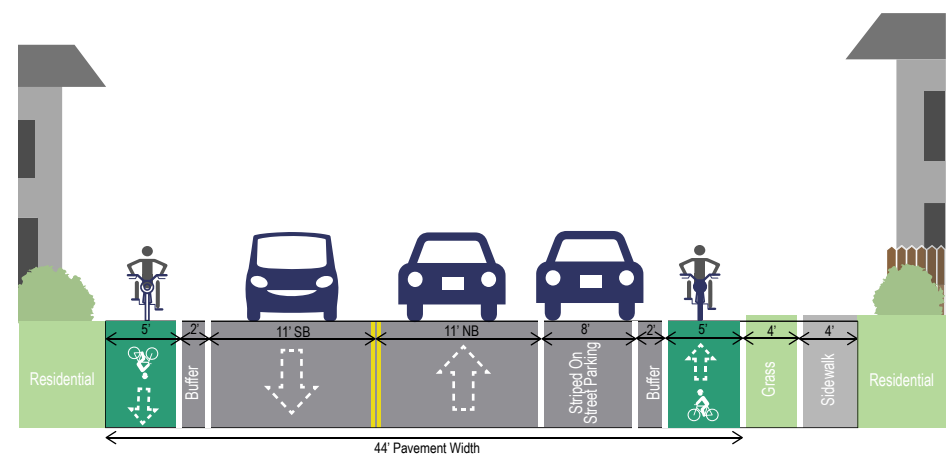


Proposed Improvements

Poplar Avenue through Riverdale Road - Alternative A



Poplar Avenue through Riverdale Road - Alternative B



Wanaque Avenue

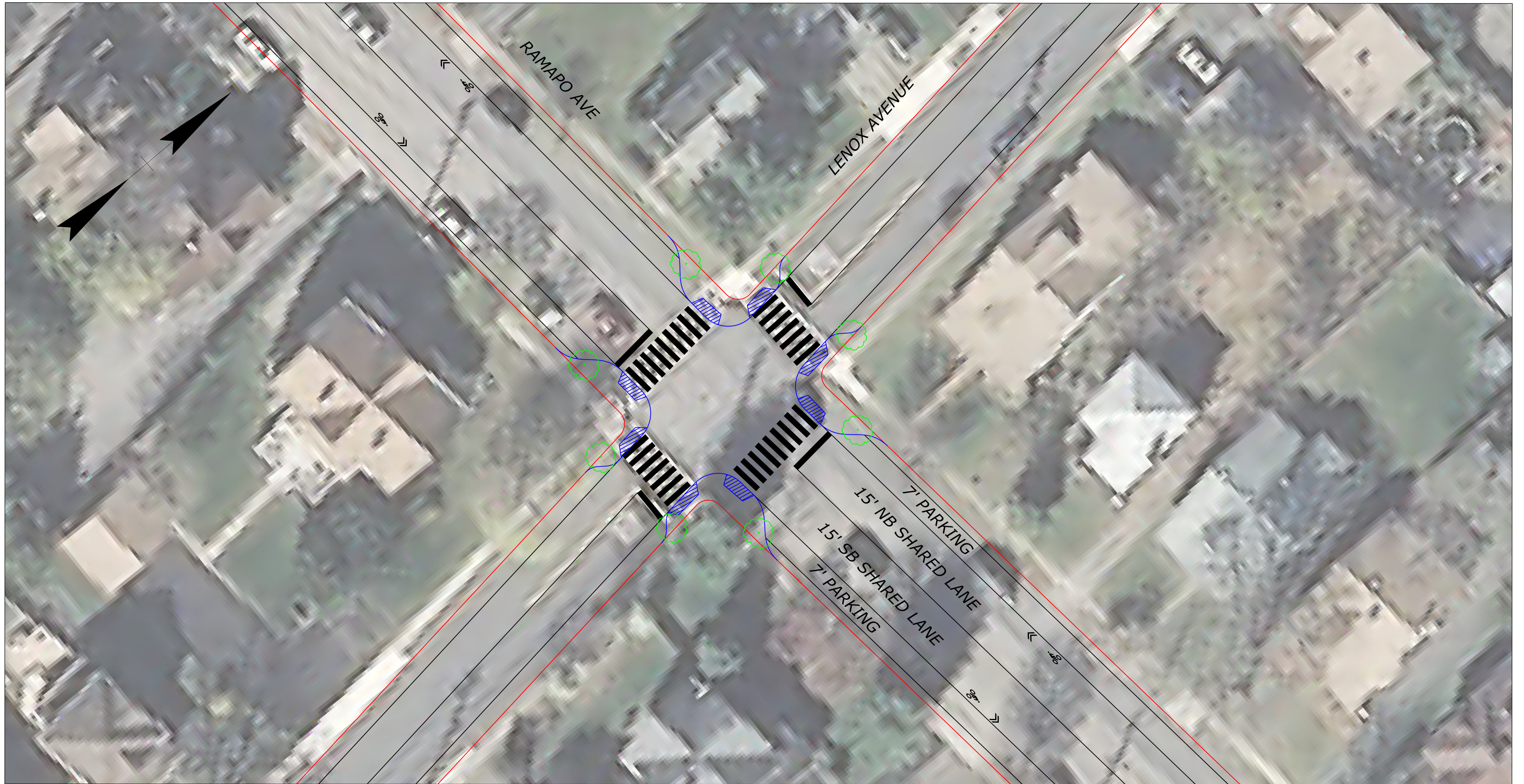
Jurisdiction: County
Speed Limit: 25 MPH

Traffic Volume (2012): 14,808
On-Street Parking: Yes




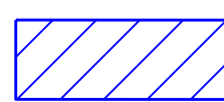
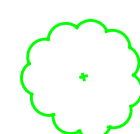
Existing Conditions	Proposed Improvements
<p>Section 1: Paterson-Hamburg Turnpike/Passaic Avenue to Pompton Avenue Length: 0.14 Mile</p>	<p>Paterson-Hamburg Turnpike/Passaic Avenue to Pompton Avenue Alternative</p>
<p>Section 2: Pompton Avenue to Lakeside Avenue Length: 0.35 Mile</p>	<p>Pompton Avenue to Lakeside Avenue - Alternative</p>
<p>Section 3: Lakeside Avenue to Ringwood Place Length: 0.24 Mile</p>	<p>Lakeside Avenue to Ringwood Place - Alternative</p>

APPENDIX C

PROPOSED INTERSECTION IMPROVEMENTS



LEGEND

-  PROPOSED CURBLINE
-  PROPOSED PAVEMENT MARKINGS
-  EXISTING CURBLINE
-  ADA-COMPLIANT CURB RAMP
-  TREE



Conceptual Alternatives

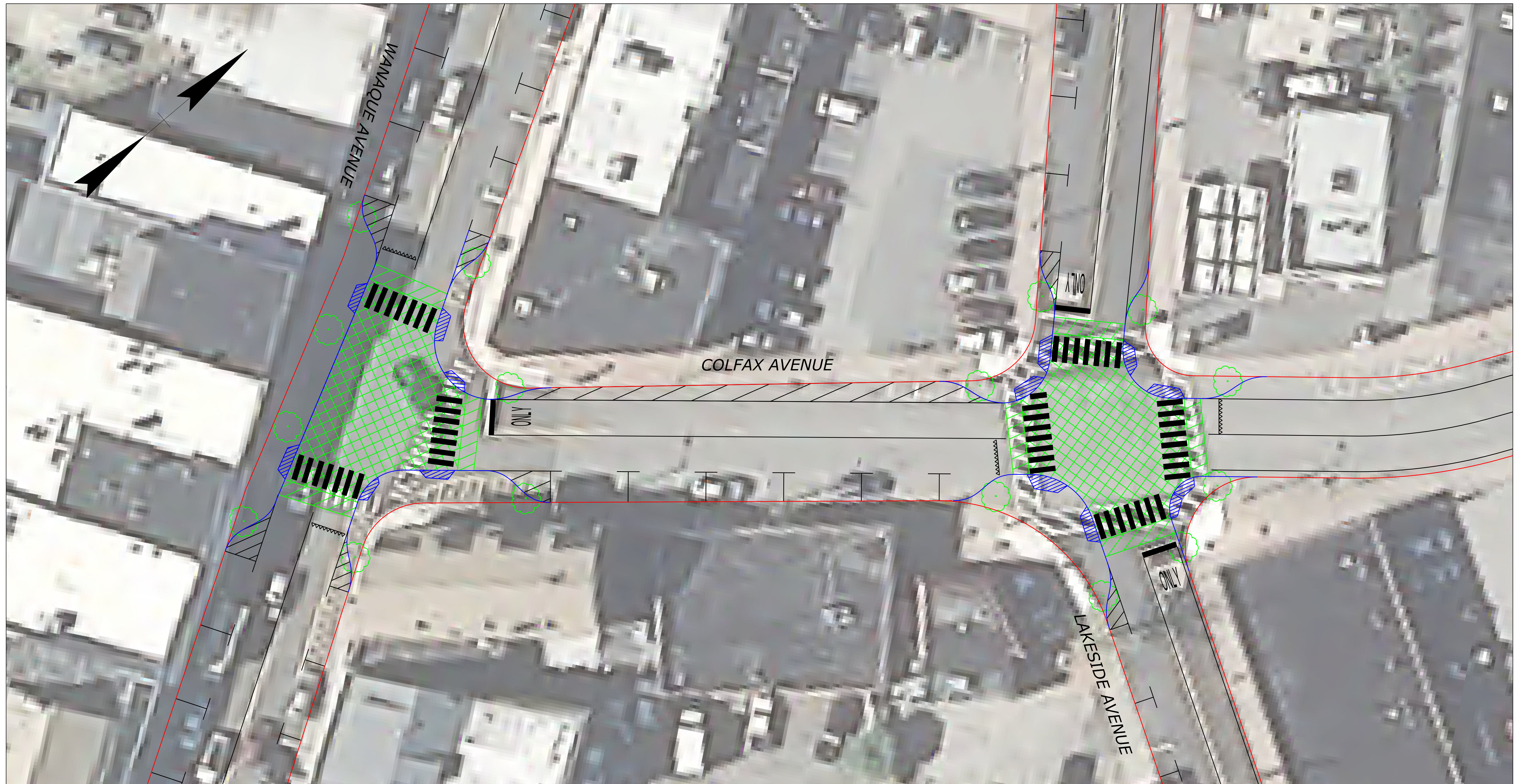
Borough of Pompton Lakes
Pedestrian and Bicycle Improvement Project

Location Ramapo Avenue and Lenox Avenue



June 2018

Michael Baker
INTERNATIONAL

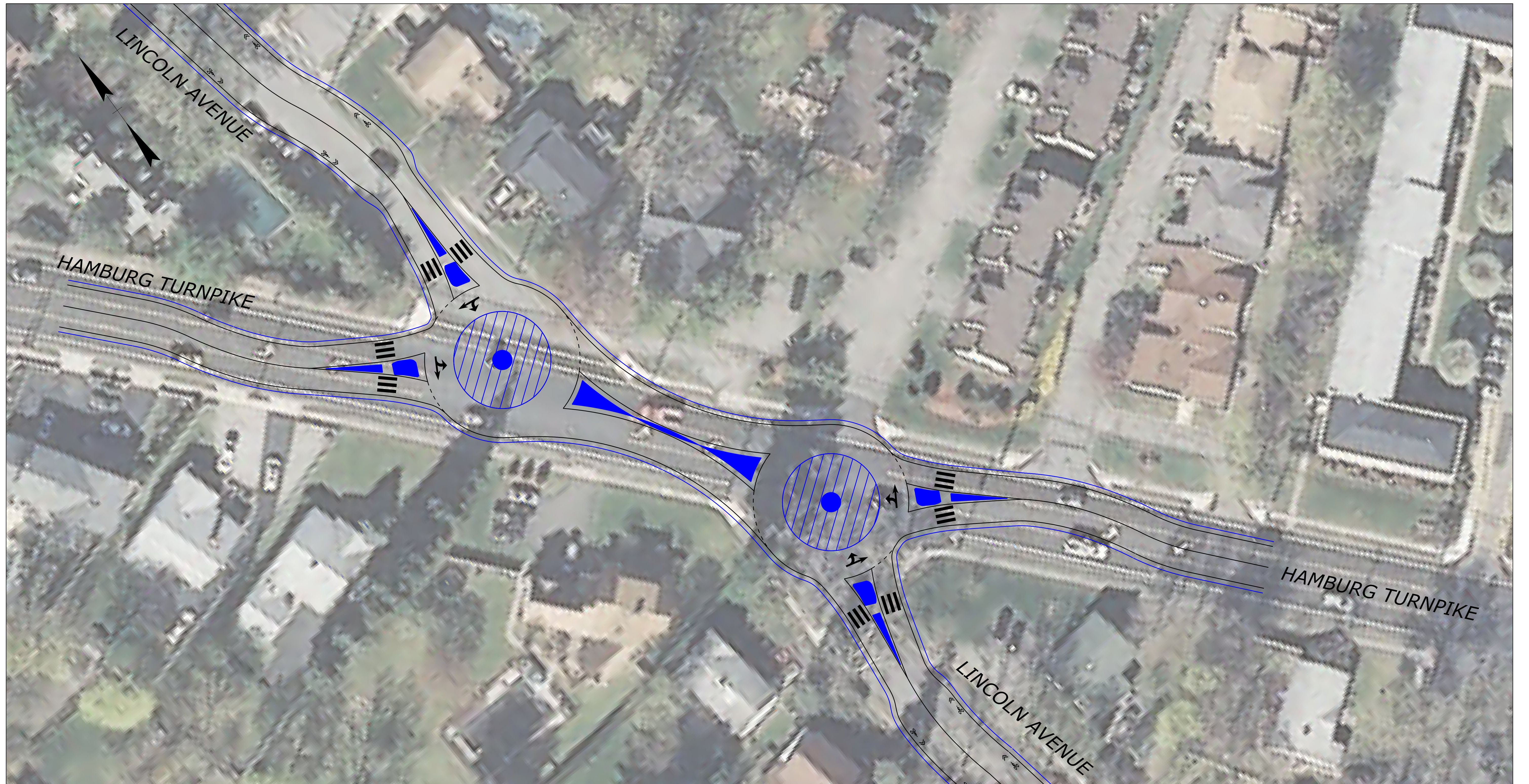


LEGEND



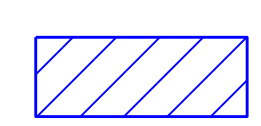

- PROPOSED CURBLINE
- PROPOSED PAVEMENT MARKINGS
- EXISTING CURBLINE
- ADA-COMPLIANT CURB RAMP
- RAISED INTERSECTION
- RAISED INTERSECTION RAMP
- TREE



<p>Conceptual Alternatives Borough of Pompton Lakes Pedestrian and Bicycle Improvement Project</p>	
Location	Wanaque Avenue and Colfax Avenue
<p>June 2018</p>	
	<p>Michael Baker INTERNATIONAL</p>



LEGEND

-  PROPOSED CURBLINE
-  PROPOSED PAVEMENT MARKINGS
-  MOUNTABLE PAVEMENT
-  RAISED CURB



Conceptual Alternatives

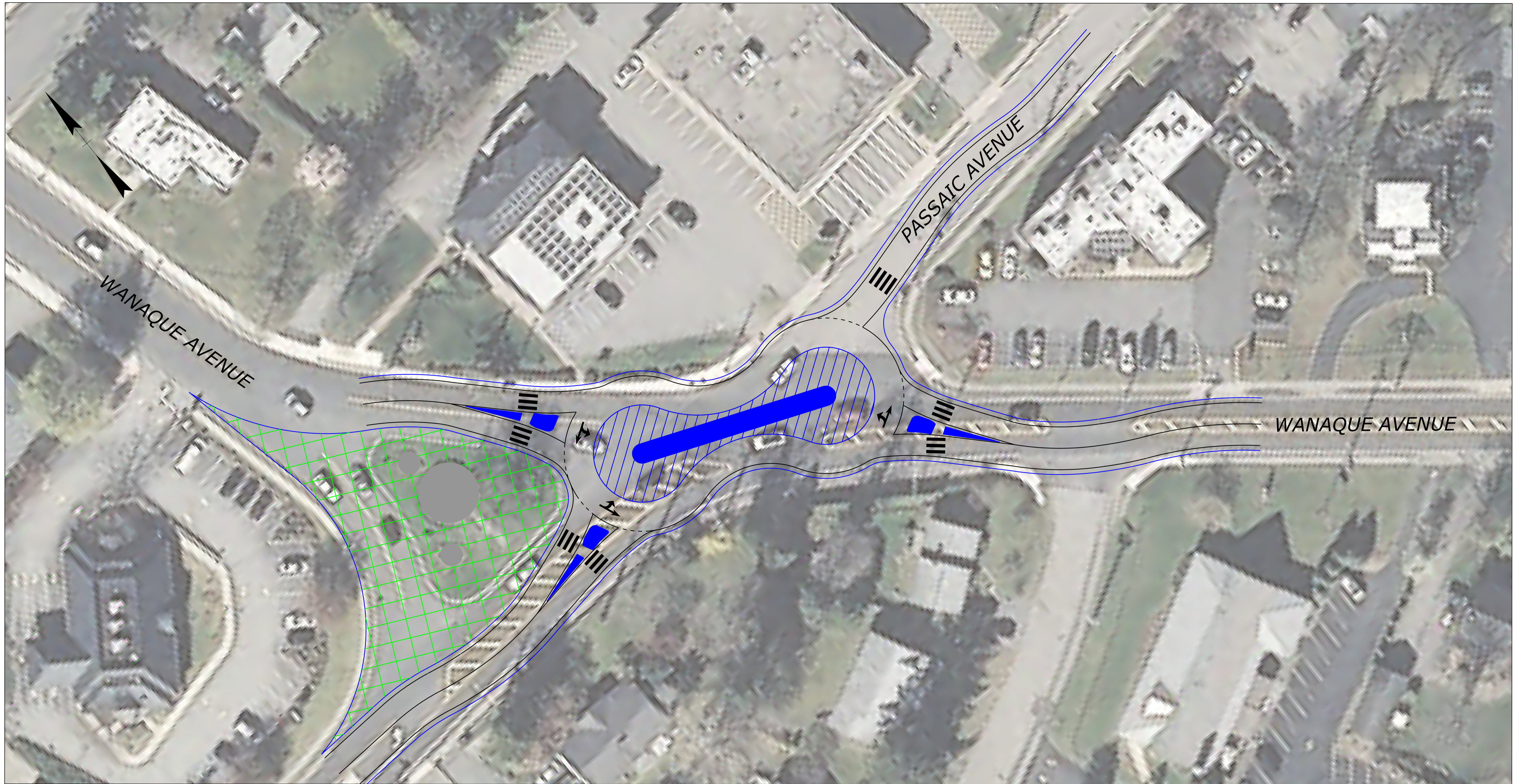
Borough of Pompton Lakes
Pedestrian and Bicycle Improvement Project

Location	Hamburg Turnpike and Lincoln Avenue
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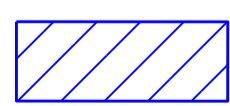

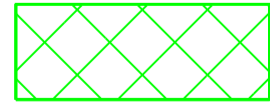



June 2018



Michael Baker
INTERNATIONAL



LEGEND

-  PROPOSED CURBLINE
-  PROPOSED PAVEMENT MARKINGS
-  MOUNTABLE PAVEMENT
-  RAISED CURB
-  PROPOSED GRASS AREA
-  EXISTING STATUES



<p>Conceptual Alternatives Borough of Pompton Lakes Pedestrian and Bicycle Improvement Project</p>	
Location	Wanaque Avenue and Passaic Avenue
	
<p>June 2018</p>	
	

APPENDIX D

BICYCLE COMPATIBILITY RATING CRITERIA TABLES

Bicycle Compatibility Rating Criteria
(Based on Traffic Volume, Speed and Roadway Geometry)
Rural

One-way Daily Traffic Volumes

Rural

1,200 - 2000
Vehicles/Day

Posted Speed (MPH)	Roadway Geometry										
	Shared							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Green	Green	Green	Green	Green	Green	Green	Green	Green	Green	Green
30	Green	Green	Green	Green	Green	Green	Green	Green	Green	Green	Green
35	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green	Green
40	Red	Red	Red	Red	Red	Yellow	Yellow	Green	Green	Green	Green
45	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green
50	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green
55	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green

2,001 - 5,000
Vehicles/Day
or
2,001 - 10,000
Vehicles/Day with
Trucks Banned

Posted Speed (MPH)	Roadway Geometry										
	Shared							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Red	Yellow	Yellow	Green	Green	Green	Green	Green	Green
30	Red	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green
35	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green	Green
40	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green
45	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green
50	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green
55	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green

5,001 - 10,000
Vehicles/Day
NO Truck Ban

Posted Speed (MPH)	Roadway Geometry										
	Shared							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
30	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow
35	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
40	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
45	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
50	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
55	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow

More than 10,000
Vehicles/Day

Posted Speed (MPH)	Roadway Geometry										
	Shared							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
30	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow
35	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
40	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
45	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
50	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
55	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow

Key:



Most Suitable

Most suitable for on-road cycling. A majority of cyclists would find conditions favorable.



Moderately Suitable

Moderately suitable for on-road cycling. Cyclists of lesser skill and experience may find conditions unfavorable.



Least Suitable

Least suitable for on-road cycling. Cyclists of advanced skill and experience may find conditions unfavorable.

Bicycle Compatibility Rating Criteria
(Based on Traffic Volume, Speed and Roadway Geometry)
Urban No Parking

One-way Daily Traffic Volumes

Urban No Parking

1,200 - 2000 Vehicles/Day

Posted Speed (MPH)	Roadway Geometry										
	Shared							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Green	Green	Green	Green	Green	Green	Green	Green	Green	Green	Green
30	Red	Red	Yellow	Yellow	Green	Green	Green	Green	Green	Green	Green
35	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green	Green
40	Red	Red	Red	Red	Green	Green	Green	Green	Green	Green	Green
45	Red	Red	Red	Red	Red	Green	Green	Green	Green	Green	Green
50	Red	Red	Red	Red	Red	Red	Green	Green	Green	Green	Green
55	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green

2,001 - 5,000 Vehicles/Day or
 2,001 - 10,000 Vehicles/Day with Trucks Banned




Posted Speed (MPH)	Roadway Geometry										
	Shared							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Yellow	Yellow	Green	Green	Green	Green	Green	Green	Green
30	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green	Green
35	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green
40	Red	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green
45	Red	Red	Red	Red	Red	Red	Yellow	Green	Green	Green	Green
50	Red	Red	Red	Red	Red	Red	Red	Green	Green	Green	Green
55	Red	Red	Red	Red	Red	Red	Red	Red	Yellow	Green	Green

5,001 - 10,000 Vehicles/Day
 NO Truck Ban

Posted Speed (MPH)	Roadway Geometry										
	Shared							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
30	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
35	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
40	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow
45	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
50	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red
55	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red

More than 10,000 Vehicles/Day

Posted Speed (MPH)	Roadway Geometry										
	Shared							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
30	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow
35	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
40	Red	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow
45	Red	Red	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow
50	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red
55	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red

- Key:**
-  **Most Suitable** Most suitable for on-road cycling. A majority of cyclists would find conditions favorable.
 -  **Moderately Suitable** Moderately suitable for on-road cycling. Cyclists of lesser skill and experience may find conditions unfavorable.
 -  **Least Suitable** Least suitable for on-road cycling. Cyclists of advanced skill and experience may find conditions unfavorable.

Bicycle Compatibility Rating Criteria
(Based on Traffic Volume, Speed and Roadway Geometry)
Urban with Parking

One-way Daily Traffic Volumes

Urban with Parking

1,200 - 2000 Vehicles/Day

Posted Speed (MPH)	Roadway Geometry										
	Shared Lane							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Green	Green	Green	Green	Green	Green	Green	Green	Green	Green	Green
30	Red	Red	Yellow	Green	Green	Green	Green	Green	Green	Green	Green
35	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green
40	Red	Red	Red	Red	Red	Green	Green	Green	Green	Green	Green
45	Red	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green
50	Red	Red	Red	Red	Red	Red	Green	Green	Green	Green	Green
55	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red

2,001 - 5,000 Vehicles/Day
or
2,001 - 10,000 Vehicles/Day with Trucks Banned




Posted Speed (MPH)	Roadway Geometry										
	Shared Lane							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green
30	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green
35	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green
40	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green	Green
45	Red	Red	Red	Red	Red	Yellow	Green	Green	Green	Green	Green
50	Red	Red	Red	Red	Red	Red	Green	Green	Green	Green	Green
55	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red

5,001 - 10,000 Vehicles/Day
NO Truck Ban

Posted Speed (MPH)	Roadway Geometry										
	Shared Lane							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
30	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
35	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
40	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow
45	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow
50	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
55	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red

More than 10,000 Vehicles/Day

Posted Speed (MPH)	Roadway Geometry										
	Shared Lane							Shoulder			
	<10'	10'	11'	12'	13'	14'	15'	3'	4'	6'	8+'
25	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
30	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
35	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow	Yellow
40	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow
45	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow	Yellow
50	Red	Red	Red	Red	Red	Red	Red	Yellow	Yellow	Yellow	Yellow
55	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red	Red

- Key:**
-  **Most Suitable** Most suitable for on-road cycling. A majority of cyclists would find conditions favorable.
 -  **Moderately Suitable** Moderately suitable for on-road cycling. Cyclists of lesser skill and experience may find conditions unfavorable.
 -  **Least Suitable** Least suitable for on-road cycling. Cyclists of advanced skill and experience may find conditions unfavorable.



APPENDIX E

CRASH DATA SUMMARY TABLE

Crash Report Summary Table

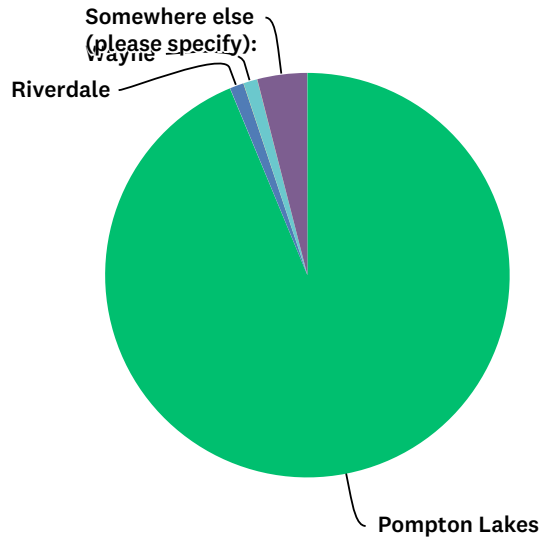
Year	Collision Type	Crash Occurred On or At	Cross Street	Contributing Circumstance #1	Contributing Circumstance #2	Weather	Surface Condition	Light	Severity
2012	Pedestrian	Wanaque Ave	500 Feet North of Lenox Ave	Brakes	Unsafe Speed	Clear	Dry	Day	Minor injury
2012	Pedalcyclist	1 Wanaque Ave	--	Driver Inattention	--	Clear	Dry	Day	PDO
2012	Pedestrian	15 Broad St	--	None	--	Clear	Dry	Day	Moderate Injury
2013	Pedalcyclist	Wanaque Ave	Colfax Ave	Improper Passing	Failed to Yield Row to Vehicle/Ped	Clear	Dry	Day	Moderate Injury
2013	Pedalcyclist	Wanaque Ave	Colfax Ave	None	--	Clear	Dry	Day	PDO
2013	Pedalcyclist	Wanaque Ave	Grove St	Failed to Yield Row to Vehicle/Ped	Other Driver/Ped Action	Clear	Dry	Dusk	Minor injury
2013	Pedalcyclist	Wanaque Ave	Lenox Ave	None	--	Overcast	Wet	Day	Minor injury
2014	Pedestrian	Ringwood Ave	Wanaque Ave	Driver Inattention	--	Clear	Dry	Dark	Minor injury
2014	Pedalcyclist	Dawes Hwy	Central Ave	Failed to Obey Traffic Control Device	--	Rain	Wet	Dark	Minor injury
2014	Pedestrian	Ringwood Ave	Willard St	None	--	Clear	Dry	Dawn	Minor injury
2014	Pedalcyclist	Hamburg Tpke	Riverview Rd	None	--	Clear	Dry	Day	Moderate Injury
2014	Pedalcyclist	Ringwood Ave	Wanaque Ave	None	--	Clear	Dry	Day	Minor injury
2014	Pedestrian	714 Hamburg Tpke	200 Feet South of Pequannock Ave	Driver Inattention	--	Clear	Dry	Day	Minor injury
2015	Pedestrian	Wanaque Ave	Babcock Pl	None	--	Overcast	Wet	Day	Minor injury
2016	Pedalcyclist	Wanaque Ave	Grove St	Driver Inattention	--	Clear	Dry	Day	Moderate Injury
2016	Pedestrian	44 Lakeside Ave	--	Driver Inattention	--	Clear	Dry	Day	PDO
2016	Pedalcyclist	Lenox Ave	Romain Ave	None	--	Clear	Dry	Day	PDO
2016	Pedalcyclist	Lakeside Ave	Legion St	Driver Inattention	Failed to Yield Row to Vehicle/Ped	Clear	Dry	Day	PDO
2016	Pedalcyclist	Hemlock Rd	Hilltop Ct	None	--	Clear	Dry	Day	Moderate Injury

APPENDIX F

PUBLIC OUTREACH SURVEY RESULTS

Q1 Where do you live?

Answered: 176 Skipped: 0

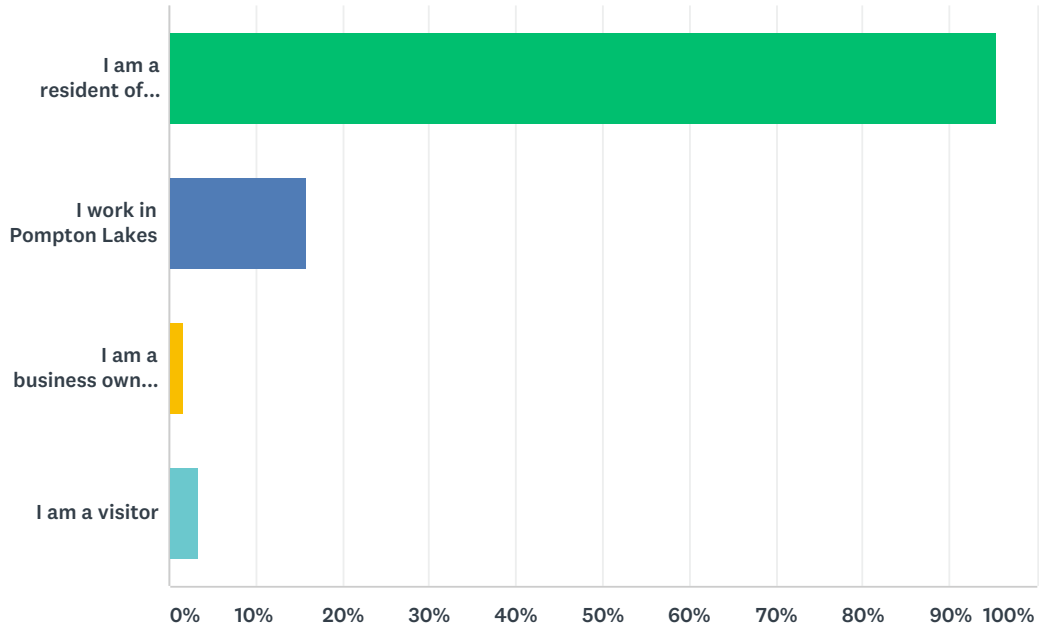


ANSWER CHOICES	RESPONSES	
Pompton Lakes	93.75%	165
Riverdale	1.14%	2
Oakland	0.00%	0
Wayne	1.14%	2
Pequannock	0.00%	0
Somewhere else (please specify):	3.98%	7
TOTAL		176

#	SOMEWHERE ELSE (PLEASE SPECIFY):	DATE
1	Recently moved to Riverdale but lived in PL for 14 years	1/24/2018 12:14 PM
2	Budd Lake	8/6/2017 4:24 PM
3	I	8/4/2017 7:00 AM
4	Bloomingtondale	8/4/2017 12:52 AM
5	Wanaque	8/1/2017 8:39 PM
6	Lincoln park	8/1/2017 8:05 PM
7	Wanaque	8/1/2017 8:03 PM

Q2 Please check all that apply to you:

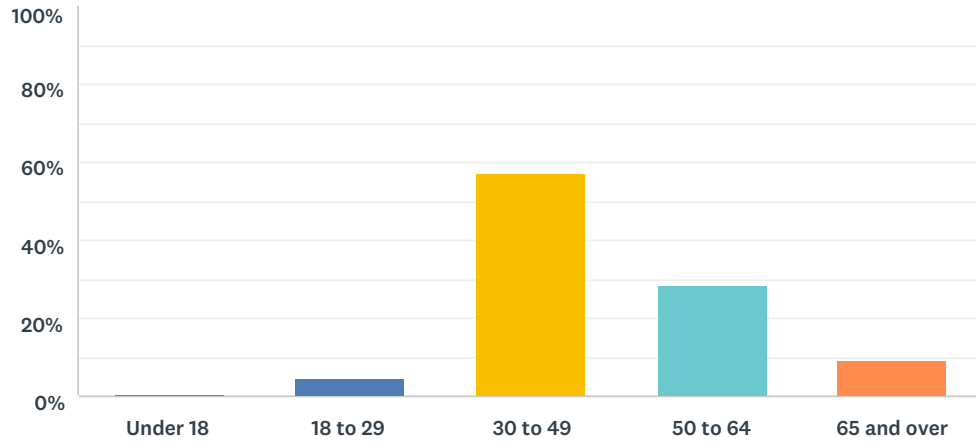
Answered: 176 Skipped: 0



ANSWER CHOICES	RESPONSES	
I am a resident of Pompton Lakes	95.45%	168
I work in Pompton Lakes	15.91%	28
I am a business owner in Pompton Lakes	1.70%	3
I am a visitor	3.41%	6
Total Respondents: 176		

Q3 How old are you?

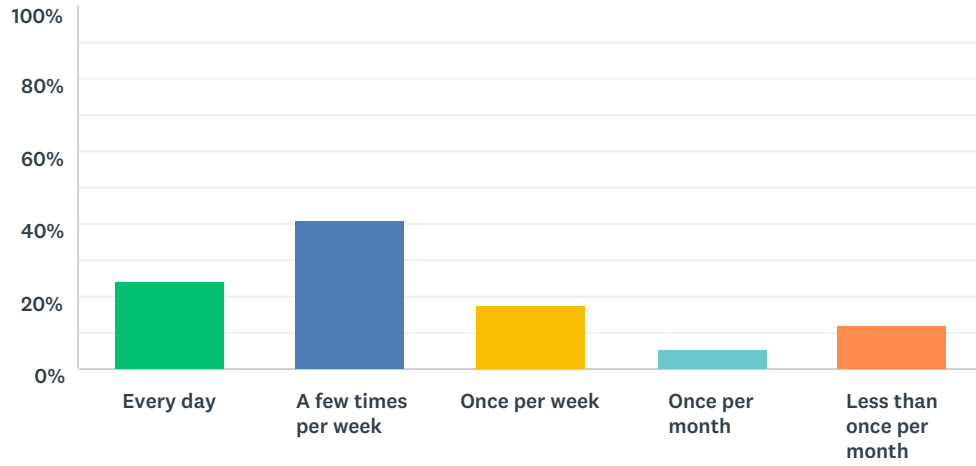
Answered: 176 Skipped: 0



ANSWER CHOICES	RESPONSES	
Under 18	0.57%	1
18 to 29	4.55%	8
30 to 49	57.39%	101
50 to 64	28.41%	50
65 and over	9.09%	16
TOTAL		176

Q4 How often do you walk or bike around Pompton Lakes?

Answered: 176 Skipped: 0



ANSWER CHOICES	RESPONSES	
Every day	23.86%	42
A few times per week	40.91%	72
Once per week	17.61%	31
Once per month	5.68%	10
Less than once per month	11.93%	21
TOTAL		176

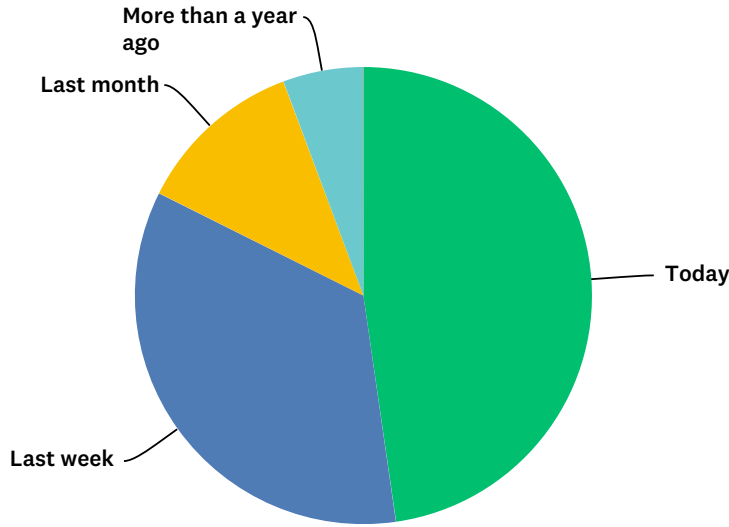
Q5 How often do you walk or bike to the following activities in Pompton Lakes?

Answered: 175 Skipped: 1

	OFTEN	SOMETIMES	NEVER	TOTAL	WEIGHTED AVERAGE
Doing errands	17.26% 29	50.00% 84	32.74% 55	168	0.00
Going to school or work	15.06% 25	16.87% 28	68.07% 113	166	0.00
Going to the park	34.68% 60	54.91% 95	10.40% 18	173	0.00
Visiting friends	27.91% 48	53.49% 92	18.60% 32	172	0.00
Going out to eat/drink	22.81% 39	42.69% 73	34.50% 59	171	0.00
Going to a transit stop	4.76% 8	18.45% 31	76.79% 129	168	0.00

Q6 When was the last time you walked more than a block in Pompton Lakes?

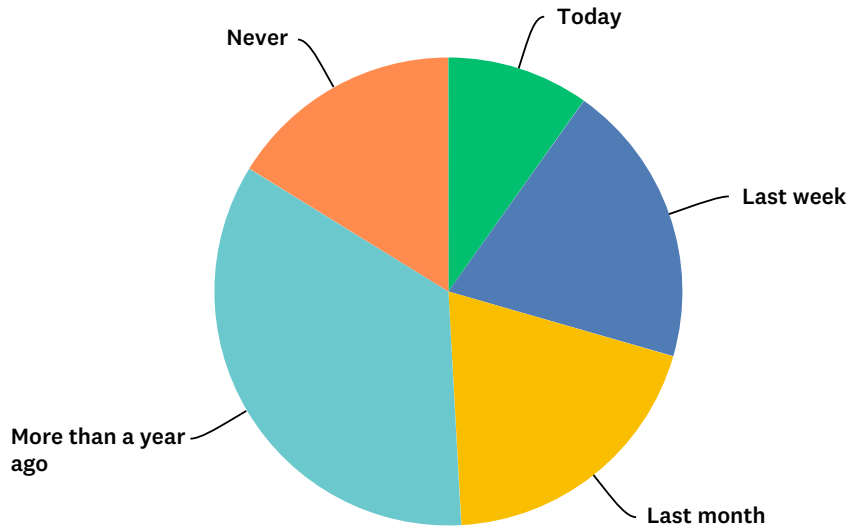
Answered: 176 Skipped: 0



ANSWER CHOICES	RESPONSES	
Today	47.73%	84
Last week	34.66%	61
Last month	11.93%	21
More than a year ago	5.68%	10
Never	0.00%	0
TOTAL		176

Q7 When was the last time you biked in Pompton Lakes?

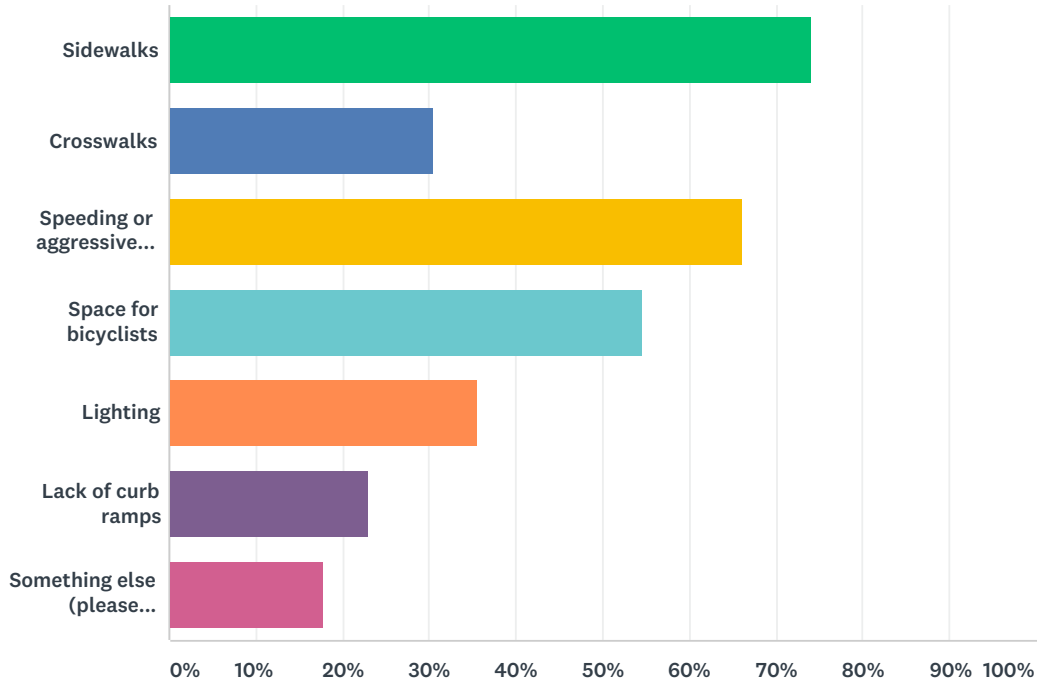
Answered: 173 Skipped: 3



ANSWER CHOICES	RESPONSES	
Today	9.83%	17
Last week	19.65%	34
Last month	19.65%	34
More than a year ago	34.68%	60
Never	16.18%	28
TOTAL		173

Q8 What are your safety concerns about walking/biking in Pompton Lakes? (check all that apply)

Answered: 174 Skipped: 2



ANSWER CHOICES	RESPONSES
Sidewalks	74.14% 129
Crosswalks	30.46% 53
Speeding or aggressive driving	66.09% 115
Space for bicyclists	54.60% 95
Lighting	35.63% 62
Lack of curb ramps	22.99% 40
Something else (please specify):	17.82% 31
Total Respondents: 174	

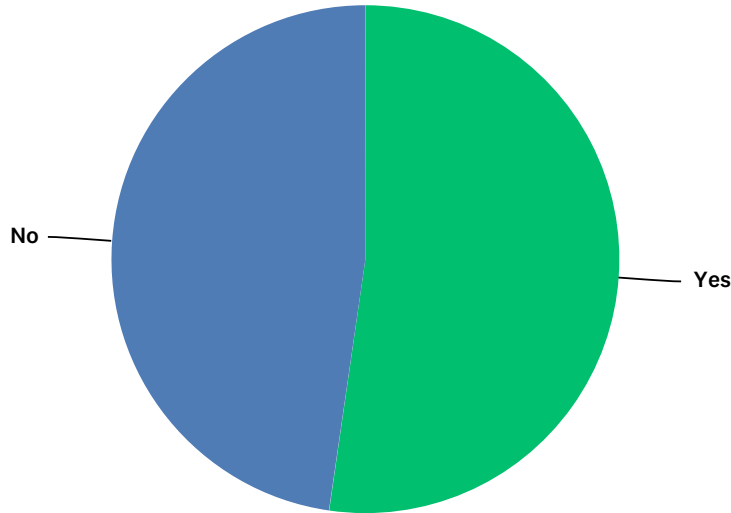
#	SOMETHING ELSE (PLEASE SPECIFY):	DATE
1	Dangerous crossings due to heavy traffic	2/27/2018 12:09 AM
2	Falling apart Bridge on West Lenox	2/21/2018 1:26 PM
3	No crossing guard on the corner of colfax and legion st. There was always a crossing guard on the corner til last year my son walks or rides his bike every day. People drive to fast on colfax. Therefore I cross my son	2/21/2018 5:27 AM
4	Ringwood Ave is dangerous for the many bikers and walkers that travel EVERY DAY back and forth at all hours! Someone is going to be killed because of a lack of sidewalks on Ringwood Ave from Hamburg TnPk to Wanaque Ave.	2/20/2018 6:22 PM
5	unsafe sidewalks	2/20/2018 4:43 PM

Pompton Lakes Complete Streets

6	Cars parked across the sidewalk, cars not stopping for people in pedestrian crosswalk	2/20/2018 1:21 PM
7	main walk arteries should be wider to accomdate passing ped traffic	2/5/2018 9:23 AM
8	No place to park and lock bikes	2/4/2018 11:17 PM
9	unsafe sidewalk or unclear from over grown bushes	1/23/2018 10:53 PM
10	Too much light at night on residential streets makes sleeping impossible without black out shades. Wrong spectrum of light. Needs to be below 4500 Kelvin	1/23/2018 5:17 PM
11	Poor condition of sidewalks for strollers	8/7/2017 10:08 AM
12	Main streets are too busy & there is no room for cyclists	8/5/2017 10:47 PM
13	Uneven sidewalk & road surfaces	8/5/2017 7:55 AM
14	Too few traffic lights.	8/5/2017 4:19 AM
15	My concern is for the children that have to walk to school. There are several sex offenders that live on Wanaque Ave that these children have to pass on a daily basis. Also I have a problem with the complex that is being forced on us, that they plan on putting where the Salvation Army is. Isn't there a place across the street from that where drug addicts go to get there drugs? Plus you will be creating more congestion in that area. Taxes will be higher and how in the world will you be able to put stores in there when you already have so many empty stores? It's disgusting what has happened to uptown.	8/4/2017 12:01 PM
16	Lack of designated bike lanes. As well as ignorant attitudes towards cyclists.	8/4/2017 9:56 AM
17	Cars parked blocking sidewalks	8/4/2017 9:19 AM
18	Low branches which force me into the street.	8/4/2017 5:39 AM
19	Seeing bicyclists without helmets or riding on the wrong side of the road	8/3/2017 10:15 PM
20	None	8/3/2017 10:09 PM
21	Sidewalks are sufficient, but as a runner can be dangerous when dark.	8/3/2017 9:58 PM
22	With all the people walking around pompton were would bicycling go with out hitting a pedestrian	8/3/2017 9:13 PM
23	High speed limits	8/3/2017 8:40 PM
24	The law is to wear a helmet and it's not being enforced enough	8/3/2017 8:27 PM
25	Lack of bike facilities	8/3/2017 8:08 PM
26	Narrow major roads	8/2/2017 10:40 AM
27	Dangerous intersection at Ramapo Ave and Hamburg Tpk (need green arrow on left going south.	8/1/2017 9:11 PM
28	Lack of bike lanes on Hamburg Turnpike	8/1/2017 7:59 PM
29	Poor inter-connectivity with other towns and between "sections" of town	8/1/2017 7:25 PM
30	Cars parked/overlapping the sidewalk forcing pedestrians to step out into the street. Cannonball-the car detailing place.	8/1/2017 6:33 AM
31	Lack of knowledge or obedience of the rules by both drivers, bicyclists and pedestrians	7/31/2017 1:02 PM

Q9 Do you view Pompton Lakes as a safe city for all road users - motorists, bicyclists, and pedestrians - to share the roads together?

Answered: 174 Skipped: 2



ANSWER CHOICES	RESPONSES	
Yes	52.30%	91
No	47.70%	83
TOTAL		174

#	IF NO, WHY NOT?	DATE
1	Roadways aren't current striped to share road with pedestrian/bicyclists	3/19/2018 3:44 PM
2	no sidewalks, lighting, stop signs	3/3/2018 11:20 AM
3	Cars parked along curbs force bikes into path of vehicles	2/27/2018 12:09 AM
4	A lot of broken up uneven sidewalks and a lot if areas aren't well lite.	2/25/2018 10:32 PM
5	Need bike lanes	2/23/2018 9:38 AM
6	Sidewalks are all messed up	2/22/2018 11:23 AM
7	Main Ave -Wanaque & by the post office is very dangerous. People open cars doors into traffic, street parking narrows road, people jay walk. The turn by the post office is blind & the street is to narrow & busy- makes driving, etc very unsafe!	2/21/2018 9:16 PM
8	Many streets are not wide enough for biking because the cars parked on the streets often force children, adults, and riding clubs to ride into traffic.	2/21/2018 1:37 PM
9	I think it has the potential with a few improvements such as better lighting, minor repairs and better signage.	2/21/2018 1:26 PM
10	Speed limit on roads is not enforced	2/21/2018 5:27 AM
11	Motorists speeding on roads and not coming to a full stop for pedestrians and stop signs	2/20/2018 10:25 PM
12	There are no or inconsistent stretches of sidewalks on Ringwood Ave the entire length from Carlough Field to Wanaque Avenue does not have a complete sidewalk system! Many children and entire families are forced to walk and rode in the streets on Ringwood Ave with cars traveling over 45 mph even thought the posted limit is 35mph.	2/20/2018 6:22 PM

Pompton Lakes Complete Streets

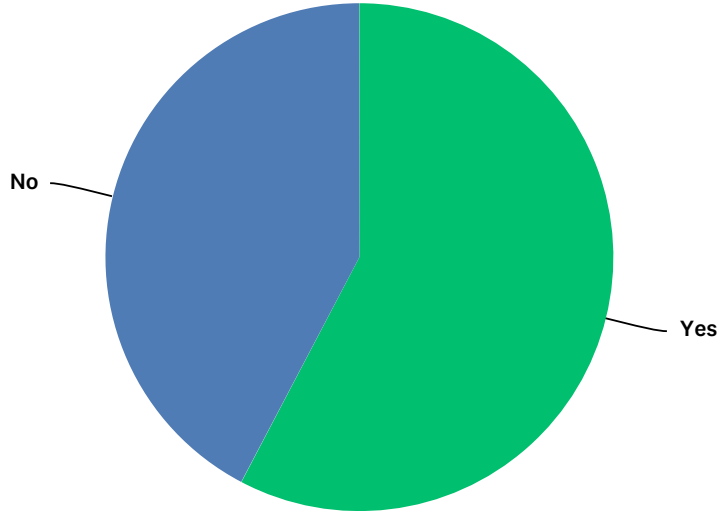
13	Wanaque is too busy	2/20/2018 6:20 PM
14	Streets are narrow with cars parked on both sides in some areas.	2/20/2018 5:27 PM
15	When I drive in the center of town and I stop for a pedestrian there's always a car behind me trying to go around me and they drive in the yellow striped areas. Also when I healed for a walker that crosses Ramapo, there's always someone trying to pass me.	2/20/2018 4:39 PM
16	Slde roads are OK. But getting into town is an issue.	2/20/2018 3:44 PM
17	Unsafe sidewalks, poor road surfaces, overgrown bushes at corners	2/20/2018 1:40 PM
18	insufficient lighting-minimal streetlights	2/20/2018 1:22 PM
19	danger from having to walk into street and cars speeding and not yeilding to pedestrian in marked crosswalk	2/20/2018 1:21 PM
20	We need sidewalks and speed signs	2/20/2018 1:15 PM
21	Not enough sidewalks available or bike paths. Lots of uneven paths	2/20/2018 1:13 PM
22	Not enough lighting to be safe at night. Too many aggressive drivers that assume they always have the right of way no matter what.	2/6/2018 12:20 PM
23	usually yes	2/5/2018 9:23 AM
24	Motorists speed around town and downtown, not enough pedestrian crosswalks and motorists are inattentive on Wanaque.	1/31/2018 10:10 PM
25	Speed is a concern	1/26/2018 11:11 PM
26	Too much speeding in town, too many parked cars along the streets	1/26/2018 7:58 AM
27	There is a lot of speeding cars especially on Ramapo Ave. There are also groups of bicyclists on weekends mostly that ride side by side taking up lanes of traffic and making it unsafe for motorists as well as themselves. Police should enforce sharing the road but cyclists need to stop riding side by side.	1/24/2018 1:42 PM
28	There is a lot of speeding and not any space to safely bike on main streets	1/23/2018 7:42 PM
29	Lack of signage	1/23/2018 7:00 PM
30	Cars are always speeding and drivers are always texting!	1/23/2018 6:59 PM
31	Drivers are texting.	1/23/2018 6:25 PM
32	Speeding on Colfax Ave	1/23/2018 6:07 PM
33	Too many people use the side streets to cut through to get to the main streets	1/23/2018 5:49 PM
34	Cars tend to forget there are pedestrians	1/23/2018 5:10 PM
35	Drivers are very aggressive down colfax and do not stop at cross walks to let people cross	11/11/2017 11:28 AM
36	Certain areas, like Ringwood Ave, lack sufficient sidewalks, space for bikes, well marked crosswalks and speeding is common.	8/11/2017 9:05 AM
37	Speeding is an issue	8/7/2017 9:18 PM
38	Cars aren't parked close enough to curb. The sidewalks are unacceptable to push a stroller on, resulting in the need to walk in the street. There needs to be a bike lane. Cars need to actually stop not just slow down when someone is waiting at a crosswalk.	8/7/2017 10:08 AM
39	Sidewalks needed and safer crossing on Riverdale Rd at curve and Riverdale Blvd & Dawes	8/7/2017 9:03 AM
40	Streets are so bumpy and messed up it's hard to bike with the family and no sidewalks	8/7/2017 8:23 AM
41	There are few bike lanes for my children and I to use and the sidewalks are terrible!	8/6/2017 3:08 PM
42	There are few bike lanes for my children and I to use and the sidewalks are terrible!	8/6/2017 2:27 PM
43	Need more bike paths or sidewalks and speeding drivers near carlough field during football	8/5/2017 10:54 PM
44	Same as above - not enough room, too busy	8/5/2017 10:47 PM
45	Mostly due to insufficient lighting, drivers speeding, and uneven sidewalk & road surfaces.	8/5/2017 7:55 AM
46	Too dark.	8/5/2017 4:19 AM

Pompton Lakes Complete Streets

47	Not enough light at night and cars don't share the road.	8/4/2017 10:26 PM
48	Many people do not obey 25MPH speed limit. And on lakeside where many children walk to school, there are few sidewalks and many blind spots.	8/4/2017 3:39 PM
49	See above	8/4/2017 12:01 PM
50	Cars, no sidewalks,	8/4/2017 10:31 AM
51	Poor dangetous attitudes by motorist towards people on bikes.	8/4/2017 9:56 AM
52	Roads are not wide enough, too much debre in the roadway and too many abrasive drivers	8/4/2017 9:19 AM
53	When walking, most cars do not stop at pedestrian walkways even though they are marked.	8/4/2017 9:08 AM
54	To a certain degree. I think it's safer from someone walking as opposed to someone riding a bike	8/4/2017 8:37 AM
55	Not on Wanaque Ave, there is not a bike option there with the street parking.	8/4/2017 6:33 AM
56	Many jay-walkers on Wanaque Avenue, speeding through residential areas, near playgrounds.	8/3/2017 10:15 PM
57	Would love a bit more space so I can take my children with me. Drivers can be a bit fast.	8/3/2017 9:58 PM
58	Not enough space on the street or sidewalks to safely ride	8/3/2017 9:46 PM
59	See above	8/3/2017 9:41 PM
60	To much traffic and no place for the pedestrians to walk or bike	8/3/2017 9:13 PM
61	Too much speeding and not enough care for other users of the road...even pedestrians in crosswalks	8/3/2017 8:40 PM
62	Qualifier, I live on Lincoln just off of Hamburg Tpk. People peel off of Hamburg going 40 and don't slow down until they make the turn at Corning, if at all. I cross Lincoln when walking my dog and am always nervous a car will come speeding through the intersection.	8/3/2017 8:37 PM
63	Bicycles are always in the roadway instead of single file near the curb. Makes it difficult for drivers to pass. Speeding is rampant in our town especially colfax avenue.	8/3/2017 8:21 PM
64	Too many speeders	8/3/2017 8:13 PM
65	The missing sidewalks on Ringwood Ave make it dangerous	8/3/2017 8:11 PM
66	On many roads, drivers go too fast	8/3/2017 8:08 PM
67	No space for bicyclist	8/2/2017 7:20 PM
68	Need more access	8/2/2017 11:55 AM
69	Most areas yes but major roads some have no sidewalks and speeders	8/2/2017 10:40 AM
70	With sporadic exceptions	8/1/2017 9:11 PM
71	Walkers need side walks	8/1/2017 9:10 PM
72	Very good, but not what it should be	8/1/2017 7:25 PM
73	The bicyclists make it hazardous by riding in clumps instead of single file.	8/1/2017 6:33 AM
74	IE: Colfax ave certain cars park up in the sidewalk forcing pedestrians into the street to move around.	8/1/2017 6:29 AM
75	See previous response-cars speeding well above the speed limit, and lack of knowledge or obedience of the rules by drivers, bicyclists, and pedestrians	7/31/2017 1:02 PM
76	Sidewalks in town are a mess and drivers, especially on colfax, are dangerous	7/31/2017 10:42 AM
77	Areas of Pompton cannot be safely biked.	7/31/2017 10:28 AM

Q10 Do you feel that children in Pompton Lakes can walk safely to and from schools or parks?

Answered: 175 Skipped: 1



ANSWER CHOICES	RESPONSES	
Yes	57.71%	101
No	42.29%	74
TOTAL		175

#	IF NO, WHY NOT?	DATE
1	more crossguards	3/3/2018 11:20 AM
2	Too few safe and guarded crossings	2/27/2018 12:09 AM
3	Same as above.	2/25/2018 10:32 PM
4	Need side walks on all street and bike lanes on main streets	2/23/2018 9:38 AM
5	Missing sidewalks n fast drivers	2/22/2018 4:18 PM
6	Sidewalks and cars are always driving crazy	2/22/2018 11:23 AM
7	not safe anywhere	2/22/2018 8:53 AM
8	The bridge by the old a&p is unsupervised for children to walk by & drivers speed there - not safe!	2/21/2018 9:16 PM
9	My son broke both of his arms in a bicycle crash on his way down to summer camp two years ago. The cars coming off of Ramapo Ave turn that corner way to fast and often force the children to swerve out of the way or into the pack of other bikers.	2/21/2018 1:37 PM
10	The park isn't close to my part of town. Some streets require better signage.	2/21/2018 1:26 PM
11	Safety is not a concern for children walking to school	2/21/2018 5:27 AM
12	Same reason above	2/20/2018 10:25 PM
13	See above	2/20/2018 6:22 PM
14	some areas, due to sidewalks being missing, are unsafe. Crossing guards are not always on duty.	2/20/2018 4:43 PM
15	I only live four blocks away from Lakeside, my daughter says it's very unsafe in the mornings to cross Ramapo off of Pompton Avenue.	2/20/2018 4:39 PM

Pompton Lakes Complete Streets

16	My kids would need to cross Hamburg Turnpike	2/20/2018 3:44 PM
17	The bend on Riverdale Road is treacherous with speeding cars. I am hesitant to allow my child to walk and cross the street there without me.	2/20/2018 1:51 PM
18	upgrade the roads, sidewalks,	2/20/2018 1:40 PM
19	I've witnessed a car almost hit my child who was in a marked pedestrian crosswalk by the high school	2/20/2018 1:21 PM
20	lack sidewalks	2/20/2018 1:15 PM
21	Especially by DuPont working on the lake.	2/20/2018 1:13 PM
22	See above	2/6/2018 12:20 PM
23	Inadequate crosswalks and sidewalks to the parks.	2/3/2018 8:24 AM
24	Too many cars, too few streets, already congested.	1/31/2018 10:10 PM
25	Speeding	1/26/2018 11:11 PM
26	There are not complete sidewalks in the paths they have to walk.	1/26/2018 7:58 AM
27	Speeding cars	1/24/2018 2:41 PM
28	Depending on location of home or school or park, yes. For example, my children can safely walk to Hershfield park without having to cross a major road. But to school, they would have to cross Ramapo which as mentioned before has many speeding cars.	1/24/2018 1:42 PM
29	no sidewalk by Lakeside park	1/23/2018 9:57 PM
30	Ringwood Ave is a huge concern. People fly down the street and there are no sidewalks in my area of Ringwood Ave	1/23/2018 9:40 PM
31	Not enough sidewalks and cars don't yield to pedestrians in crosswalks.	1/23/2018 7:42 PM
32	Roads are too tight when Shared with bikes.	1/23/2018 7:35 PM
33	Same as above	1/23/2018 6:59 PM
34	Drivers texting	1/23/2018 6:25 PM
35	Too many places without sidewalks or cross walks and too many motorists that don't follow the rules	1/23/2018 5:49 PM
36	It depends how far the school or park is. Also depends on the child's age	1/23/2018 5:16 PM
37	Schools yes, because of crossing guards. Parks- only when there is no rush hour traffic.	1/23/2018 5:10 PM
38	Drivers do not stop at cross walks	11/11/2017 11:28 AM
39	Being in the downtown area walking is very safe, but if they have to cross Ringwood Ave or Hamburg Tpke it is much less so.	8/11/2017 9:05 AM
40	Busy corners and fast drivers	8/7/2017 10:19 PM
41	Yes & no...same streets mentioned above are not safe	8/7/2017 9:03 AM
42	There are no sidewalks, people are always speeding down lakeside, Jefferson, & Ramapo there should be a crossing guard on lakeside	8/7/2017 8:23 AM
43	Wanaque Ave is too busy & the lodge by the bridge is uncomfortable for me to pass by	8/5/2017 10:47 PM
44	Crossing guard are done by 4:00 sometimes kids stay later for extra help	8/5/2017 9:16 AM
45	Mostly due to aggressive and/or speeding drivers.	8/5/2017 7:55 AM
46	People speeding and not enough crosswalk	8/4/2017 10:26 PM
47	Most places but not all.	8/4/2017 3:39 PM
48	No sidewalks on all of Lakeside Ave	8/4/2017 2:44 PM
49	See answer to number 8	8/4/2017 12:01 PM

Pompton Lakes Complete Streets

50	Not always. I often see cars parked up on side walks while literally seeing police drove by without a care. I have Seen children and elderly people have to walk into the street because vehicles are parked on sidewalks. Unacceptable. At some point the toen will probably get sued by someone who was hit by a car because they had to step in the street forced by a vehicle park in the sidewalk.	8/4/2017 9:56 AM
51	Not all the time. Too many aggressive drivers and some sidewalks are terrible	8/4/2017 9:19 AM
52	Too much speeding and most cars don't let pedestrians cross in crosswalks. Lots of texting and driving too.	8/4/2017 9:08 AM
53	Somewhat	8/4/2017 8:37 AM
54	people speed and aggressively drive	8/4/2017 7:44 AM
55	Depends on area. However more crossing guards are needed in areas such as colfax and lakeside. Colfax and Wanaque Ringwood and west lenox	8/3/2017 10:27 PM
56	Only when crossing guards are on duty	8/3/2017 10:15 PM
57	It depends on where they are coming from. Ringwood Ave is very unsafe for walkers/bicycle riders	8/3/2017 9:46 PM
58	Not enough crosswalks	8/3/2017 9:13 PM
59	Cars don't slow for those in crosswalks, no yielding on turns, etc.	8/3/2017 8:40 PM
60	With crossing guards at major crossings.	8/3/2017 8:37 PM
61	Crossing streets	8/3/2017 8:26 PM
62	See below	8/3/2017 8:21 PM
63	Lack of sidewalks on Ringwood Ave	8/3/2017 8:11 PM
64	Motorists don't stop at crosswalks	8/2/2017 7:20 PM
65	Not now, construction, lack of sidewalks	8/2/2017 11:55 AM
66	With sporadic exceptions	8/1/2017 9:11 PM
67	Need side walks	8/1/2017 9:10 PM
68	Not everywhere	8/1/2017 8:07 PM
69	Drivers drive too fast, do not yield to pedestrians	8/1/2017 7:05 PM
70	See previous repsonse	7/31/2017 1:02 PM
71	As a parent I am hesitant to even take my kids for a walk or bike ride because sidewalks are a mess and cars drive recklessly	7/31/2017 10:42 AM
72	Unsure. I do not have children.	7/31/2017 10:28 AM

Pompton Lakes Complete Streets

Q11 On which streets do you feel unsafe walking or biking?

Answered: 137 Skipped: 39

#	RESPONSES	DATE
1	Hamburg Turnpike, Wanaque Avenue	3/19/2018 3:44 PM
2	Ringwood ave.	3/3/2018 11:20 AM
3	Parts of wanaque ave near the library. Poorly lit. Dawes Highway same reason and speeding. Riverdale blvd. Ramapo ave	2/28/2018 2:48 PM
4	All	2/27/2018 12:09 AM
5	Ramapo, Lakeside, Colfax, Van, Romain, and Pompton Avenues.	2/25/2018 10:32 PM
6	Hamburg and Pompton Ave between Lincoln and Van. Also, Lincoln should have a bike lane for the kids to and from school for Lakeside, Lenox and HS	2/23/2018 9:38 AM
7	Lakeside Colfax	2/22/2018 4:18 PM
8	Jefferson, Lakeside, Wanaque	2/22/2018 11:23 AM
9	riveredge	2/22/2018 8:53 AM
10	Wanaque Ave. Ringwood Ave. Lakeside Ave by the high school/post office	2/21/2018 9:16 PM
11	Ramapo, Hamburg Turnpike, and Colfax because the traffic tends to be heavier and the cars travel faster on those roads. In some spots the children ride their bikes in the street or cross between streets instead of at a corner.	2/21/2018 1:37 PM
12	Wanaque Ave because it's a very busy street, Ringwood and Hamburg Turnpike.	2/21/2018 1:26 PM
13	Dawes highway	2/21/2018 7:40 AM
14	Colfax and wanaque all streets are unsafe	2/21/2018 5:27 AM
15	Wanaque Ave., Ringwood Ave., Paterson-hamburg Tpk.	2/21/2018 12:00 AM
16	Wanaque Ave, Colfax Ave and Lakeside	2/20/2018 10:53 PM
17	Colfax, Wanaque Avenue and ramapo	2/20/2018 10:25 PM
18	RINGWOOD AVE	2/20/2018 6:22 PM
19	Wanaque, Ramapo, Colfax	2/20/2018 6:20 PM
20	The unlit streets, streets without sidewalks, and biking and driving is dangerous on narrow streets with cars parked on both sides.	2/20/2018 5:27 PM
21	Lakeside Avenue, Wanaque Avenue (biking), Ringwood Avenue	2/20/2018 4:43 PM
22	Ramapo, Lincoln Avenue, and of course want to Q Ave. In the mornings and when schools out. I also noticed that when I walk in the center of town or towards the end of the south side of town near George's deli it's very difficult to cross the street.	2/20/2018 4:39 PM
23	Hamburg Turnpike Area of Wanaque Avenue by cannon/corner of Wanaque and Hamburg Turnpike	2/20/2018 3:44 PM
24	Unsafe by the hotel across from the Starbucks.	2/20/2018 3:32 PM
25	Lakeside	2/20/2018 2:39 PM
26	Riverdale Road	2/20/2018 1:51 PM
27	I live off Ringwood Ave, and it is very dangerous to walk or bike on the road. There are no sidewalks in some parts, no room for bicycles to safely ride on the road in some areas, and cars drive very fast. We would definitely bike and walk more if that road was safer.	2/20/2018 1:46 PM
28	hamburgh turnpike	2/20/2018 1:40 PM
29	Hamburg Turnpike and Wanaque Ave	2/20/2018 1:34 PM

Pompton Lakes Complete Streets

30	Lakeside, Colfax, Wanaque, Ramapo	2/20/2018 1:21 PM
31	Hamburg	2/20/2018 1:17 PM
32	South end Ringwood Ave and Gatden road to park	2/20/2018 1:15 PM
33	Lakeside, Jefferson ave. People fly down Jefferson ave like it's a highway. Not all property has sidewalks	2/20/2018 1:13 PM
34	Lakeside ave and downtown crossings.	2/6/2018 12:20 PM
35	Downtown Wanaque Ave.	2/5/2018 10:58 AM
36	Anything along Hamburg Turnpike, Ringwood Ave, and biking near or along Wanaque Ave.	2/4/2018 11:17 PM
37	Ramapo	2/3/2018 8:24 AM
38	When walking my dogs, streets with no sidewalks are a little more dangerous when I need to be more mindful of automobile traffic.	2/2/2018 2:09 PM
39	Colfax. Cannonball. The intersection of Colfax, Cannonball, and Hamburg is very dangerous, especially during times of heavy traffic	2/2/2018 11:25 AM
40	Ringwood Avenue, north of Wanaque Avenue. I often bike to the Stop and Shop and there is a stretch with no sidewalk at all. It's obviously a very busy road and not a lot of extra space, but if the sidewalk could be extended another 25 yards to the Pompton Millwork parking lot, it would help create a good bike option for that road.	2/2/2018 9:27 AM
41	Wanaque Ave.	1/31/2018 10:10 PM
42	Ringwood ave	1/26/2018 11:11 PM
43	Lakeside, Colfax, Wanaque, Hamburg, Ramapo, South End has no side walks really - Dawes highway	1/26/2018 7:58 AM
44	Summit Avenue, lakeside Avenue between lakeside Avenue and Jefferson. Pompton Avenue between Summit and Lincoln. Van Ave between Pompton Avenue and Lenox Avenue.	1/24/2018 7:29 PM
45	Hamburg tpk, colfax, lakeside, ramapo at times.	1/24/2018 1:42 PM
46	Parts of the neighborhood on the hill behind Lakeside School (mostly Summit Ave) has lighting that is not always on at night.	1/24/2018 12:14 PM
47	Riverdale Road, Colfax Ave., Wanaque Ave.	1/24/2018 9:21 AM
48	Streets with no sidewalks	1/23/2018 11:54 PM
49	colfax, jefferson triangle, parts of lakeside	1/23/2018 10:53 PM
50	Lakeside ave	1/23/2018 9:57 PM
51	Crossing Hamburg and Ramapo and Lakeside	1/23/2018 9:44 PM
52	Ringwood Avenue	1/23/2018 9:40 PM
53	Jefferson Ave.	1/23/2018 9:21 PM
54	Lakeside	1/23/2018 7:44 PM
55	Wanaque Ave., Colfax Ave., Van Ave., Lincoln Ave.	1/23/2018 7:42 PM
56	Ringwood Avenue, Wanaque Avenue, Hamburg Tpke.	1/23/2018 7:35 PM
57	Wanaque Ave Ramapo Ave. Cannonball Rd	1/23/2018 7:00 PM
58	Colfax, Lakeside. Cars are always speeding! It would be nice to have a place to walk our dogs too!	1/23/2018 6:59 PM
59	Wanaque Ave, Hamburg Turnpike, Madison Pl, Dawes Highway, Lakeside Ave	1/23/2018 6:25 PM
60	None	1/23/2018 6:07 PM
61	Lakeside Ave, Jefferson, Colfax for biking, Riverdale Road	1/23/2018 5:49 PM
62	Lakeside	1/23/2018 5:26 PM
63	Any streets without sidewalks	1/23/2018 5:17 PM
64	Hamburg tpke	1/23/2018 5:16 PM

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65	Wanaque ave by old A & P	1/23/2018 5:10 PM
66	Colfax Avenue, Lakeside Avenue	11/11/2017 11:28 AM
67	Ringwood Ave, parts of Hamburg Turnpike, and Wanaque Ave for biking	8/11/2017 9:05 AM
68	Ramapo Ave, Hamburg Turnpike, Dawes Highway bt Quick Chek, Colfax Ave	8/7/2017 10:19 PM
69	Hamburg Turnpike, Wanaque Ave	8/7/2017 10:16 AM
70	Ramapo Ave. and on the main drag of Pompton.	8/7/2017 10:08 AM
71	Riverdale rd.	8/7/2017 9:03 AM
72	Ramapo, lakeside, Jefferson	8/7/2017 8:23 AM
73	Ringwood Ave	8/7/2017 12:28 AM
74	Ringwood Ave, Wanaque Ave, Colfax Ave	8/6/2017 4:24 PM
75	Crossing Hamburg Turnpike on Ramapo. My daughter was almost hit by a driver turning left.	8/6/2017 3:08 PM
76	Crossing Hamburg Turnpike on Ramapo. My daughter was almost hit by a driver turning left.	8/6/2017 2:27 PM
77	Ringwood ave	8/5/2017 10:54 PM
78	Ringwood, wanaque, Hamburg toke	8/5/2017 10:47 PM
79	Biking on wanaque	8/5/2017 8:54 AM
80	Howard St, Dupont Place, Cannonball Rd, Babbit, Poplar, Walnut, Maple, Barbara Dr, Schuyler is where I walk daily however I sometimes walk on other streets and feel the same there with the exception of Wanaque Avenue.	8/5/2017 7:55 AM
81	Hamburg Turnpike	8/5/2017 4:19 AM
82	Colfax, Lakeview, Mandeville ... others	8/4/2017 10:26 PM
83	Colfax. People drive too fast. And bicycle groups are all too often three or four abreast not obeying traffic laws by riding single file	8/4/2017 5:18 PM
84	Lakeside	8/4/2017 3:39 PM
85	Lakeside, Hemlock,	8/4/2017 10:31 AM
86	Lakeside ave near Lakeside park is a mess. Colfax Ave after the sidewalks end.	8/4/2017 10:07 AM
87	Colfax is especially a problem and ramapo.	8/4/2017 9:56 AM
88	Colfax and lakeside and hamburg tnpk to name a few	8/4/2017 9:19 AM
89	Mostly main streets, Colfax, Ramapo, Ringwood, Wanaque etc	8/4/2017 9:08 AM
90	Ramapo Avenue, Hamburg Turnpike, lakeside Avenue, Ringwood Avenue	8/4/2017 8:37 AM
91	Lakeside. Broadway.	8/4/2017 8:35 AM
92	Hamburg tpke; wanaque ave;	8/4/2017 7:05 AM
93	Riverdale Ave would be one walking down to Joe Grill from the downtown area.	8/4/2017 7:00 AM
94	Wanaque Ave	8/4/2017 6:33 AM
95	Pompton Avenue and Romaine Avenue	8/4/2017 5:39 AM
96	Summit Ave., Cannonball Rd., Riverdale Rd., Riverview Rd., Ringwood Ave., Paterson-Hamburg Tpk.	8/3/2017 10:56 PM
97	Ringwood ave. areas with very little shoulder, areas with no sidewalk and overgrown bushes.	8/3/2017 10:27 PM
98	Ringwood ave	8/3/2017 10:23 PM
99	Cannonball (delivery trucks speeding making u turns, wide turns), Walnut St (sidewalks not safe, and cars are sometimes parked on both sides of roads with no room for bikers, let alone 2 way vehicle pass through), I have not biked on Wanaque because evening driving in a vehicle is overwhelming.	8/3/2017 10:15 PM
100	Colfax Ave	8/3/2017 10:09 PM

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101	lakeside ave colfax	8/3/2017 10:09 PM
102	I think busker streets like Dawes Highway, Colfax, and Ringwood court are not ideal.	8/3/2017 9:58 PM
103	Colfax, Ringwood and Wanaque	8/3/2017 9:58 PM
104	Hamburg Turnpike, Wanaque Ave, Riverdale Rd	8/3/2017 9:51 PM
105	Colfax, Lakeside	8/3/2017 9:49 PM
106	Ringwood Ave. There are no sidewalks in some parts, cars drive very fast, and there is very little room on the road to bike. In addition, the vegetation near Broad Street grows into the road onto Ringwood, and because of the curve and no sidewalks or space, it is extremely dangerous to ride there.	8/3/2017 9:46 PM
107	Hamburg, lakeside	8/3/2017 9:24 PM
108	Ringwood ave broad street midland ave wanaque ave	8/3/2017 9:13 PM
109	Hamburg, Ramapo, Colfax, Lincoln	8/3/2017 8:40 PM
110	None	8/3/2017 8:27 PM
111	Hamburg, Ringwood, wanaque	8/3/2017 8:26 PM
112	Crossing Hamburg tpke Ramapo because of speeding Riverview Rd bc of speeding and cut through	8/3/2017 8:23 PM
113	Hamburg Tpke	8/3/2017 8:21 PM
114	Colfax Ave and Lakeside Ave. hardly any sidewalks on Lakeside from bridge to park.	8/3/2017 8:21 PM
115	Dawes/Riverdale ave	8/3/2017 8:13 PM
116	Ringwood Ave	8/3/2017 8:11 PM
117	Ramapo Avenue, Hamburg Turnpike	8/3/2017 8:08 PM
118	Ringwood Ave when you get by the nursing home before I-287 the road narrows there	8/2/2017 11:56 AM
119	Lakeside, Colfax, side streets	8/2/2017 11:55 AM
120	Hamburg Tpke, Lakeside Ave, Ringwood Ave	8/2/2017 10:40 AM
121	Intersection at Ramapo Ave and Hamburg Tpk. Also need better street lighting on some roads (like Ramapo Ave, and Ringwood Ave). Ringwood Ave is also tougher to walk or bike on. Ramapo Avenue needs better bike lanes.	8/1/2017 9:11 PM
122	Several	8/1/2017 9:10 PM
123	Uneven , horrible sidewalks.	8/1/2017 8:26 PM
124	Jefferson Ave	8/1/2017 8:15 PM
125	Pompton	8/1/2017 8:03 PM
126	Hamburg Turnpike	8/1/2017 7:59 PM
127	Garden road	8/1/2017 7:49 PM
128	Colfax ave	8/1/2017 7:34 PM
129	Crossing Hamburg turnpike.	8/1/2017 7:25 PM
130	Ring wood ave north of Hamburg tpke and most of hamburgtpke	8/1/2017 7:20 PM
131	Promotion and Lincoln	8/1/2017 7:05 PM
132	The entire section of DuPont PI & Howard st with the speeders especially near the new pocket park. WE NEED A 4 WAY STOP SIGN !!!! Also, Cannonball and the commercial vehicles that park to take care of business - ALL OF CANNONBALL IS A NO PARKING ZONE but signs have been taken down and it is very hazardous especially at that tight curve near #81	8/1/2017 6:33 AM
133	Colfax, Lakeside, ramapo	8/1/2017 6:29 AM
134	Biking on Wanaque Ave	7/31/2017 11:44 PM
135	Colfax Ave, Lakeside Ave	7/31/2017 1:02 PM

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136	We live off Colfax and hear and see cars flying down the street all the time	7/31/2017 10:42 AM
137	My street, Ringwood Ave would benefit from more sidewalks. We would also benefit greatly from a bike path on this street.	7/31/2017 10:28 AM

Q12 What else can be done to improve the safety of pedestrians and bicyclists in Pompton Lakes?

Answered: 115 Skipped: 61

#	RESPONSES	DATE
1	Striping or barriers for bicyclists, pedestrian only zone, reducing speed limit	3/19/2018 3:44 PM
2	sidewalks, lights, signs	3/3/2018 11:20 AM
3	Fix all sidewalks, add more crosswalks, add more vehicle stops at intersections, fix or replace signage, make roundabout at Hamburg and Wanaque.	2/27/2018 12:09 AM
4	Replace or fix damaged curbing do to salt and plowing through winter months.	2/25/2018 10:32 PM
5	Add a few bicycle paths	2/22/2018 4:18 PM
6	Fix the streets, fix the sidewalks, and something about the speeding	2/22/2018 11:23 AM
7	better lighting at night. this town is so dark on the poplar side	2/22/2018 8:53 AM
8	Supervision by the bridge. No street parking in front of the post office, park in the back & make a back entrance so it's convenient for patrons.	2/21/2018 9:16 PM
9	We have a great town for walking and biking. I have been in town since 1973 and love the fact that everybody bikes and walks. Some of the sidewalks need repair (my husband fell on the corner of Colfax and Romain due to broken and uneven sidewalk). There are many places in town where the roots of trees have pushed up the sidewalk, making it uneven. I would hate to see the trees removed . Perhaps the gaps could be evened out by adding additional cement to make the sidewalks even. It would be so much easier for the children to ride on their small bikes etc, people to push strollers and pull wagons, and for elementary students to ride their bikes when there are no pedestrians. This is a great town and I love to walk to the park or to ice cream and shopping, my son bikes, scooters, and skateboards everyday, we take our grandchildren for walks, stroller rides, and to ride their small bikes, and I walk to teach at school everyday all year long. Automobile traffic needs to slow down and do the speed limit on the three busy roads and they must respect the biking and pedestrian traffic.	2/21/2018 1:37 PM
10	Better lights and signs. Clearer marked roads.	2/21/2018 1:26 PM
11	Repair sidewalks	2/21/2018 11:58 AM
12	More crossing guards and more visible signs	2/21/2018 5:27 AM
13	Periodic police details in the downtown enforcing speed limits and crosswalk laws.	2/21/2018 12:00 AM
14	Installing a bike/walk lane	2/20/2018 10:25 PM
15	Provide a bike lane	2/20/2018 6:34 PM
16	Speed bumps on Wanaque ave though the center of town	2/20/2018 6:22 PM
17	Rethink building up this wonderful quaint town!	2/20/2018 6:20 PM
18	More lighting on the side streets, and improved sidewalks.	2/20/2018 5:27 PM
19	Bike lanes, more monitoring of speeding motorists, property managers/town safety officers(?) enforcing existing legal guidelines for homeowners with sidewalks in disrepair	2/20/2018 4:43 PM
20	Bicycle lanes would help. I wish the cops would be stricter in the mornings with motorists. In the mornings, parents just drop their kids off on main streets and the kids just run into the middle of the streets to try to cross the road to get to their schools. I almost hit 2 highschoolers doing this. This also happens on Pompton Avenue with St. Mary's. It's very dangerous. If they're going to allow parents to let their kids walk and run across street in the mornings, then I feel that it's the town's responsibility to make those streets only one way to limit the amount of traffic. This way my daughter would feel safe crossing certain streets trying to get to school in the mornings and after school.	2/20/2018 4:39 PM

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21	There are no continuous sidewalks. Sometimes they just end. My kids cannot walk up Ringwood (from Carlough straight up towards HoKing) because there are no sidewalks.	2/20/2018 3:44 PM
22	Improve roadway surfaces, designated bike path	2/20/2018 1:40 PM
23	uncertain much can be done	2/20/2018 1:34 PM
24	Enforcing speed limits and right of way at marked crosswalks	2/20/2018 1:21 PM
25	Sidewalks and better lighting	2/20/2018 1:15 PM
26	Bike paths	2/20/2018 1:13 PM
27	Lighting and perhaps flashing signs.	2/6/2018 12:20 PM
28	More road marking and signs to alert drivers that bicyclists are present.	2/5/2018 10:58 AM
29	wider walk ways on main arteries ie. lakeside ave from wanaque ave to lake park. encourage families with carrigages etc to become connected with lake/park and downtown. we need small shade trees along lake walk so it wont be so hot in summer . get free trees from shade tree comission?? dont need little park by lake near school. it will be unsecured and out of visual sight from street. GREAT PLACE TO HANG OUT AT NIGHT FOR KIDS.. IT WILL BE A MESS IN A FEW MONTHS. AND ALSO A PLACE TO DO ?????**#2# thank You	2/5/2018 9:23 AM
30	Enforce a 25 mph (or lower) speed limit on Hamburg and on Colfax	2/2/2018 11:25 AM
31	Not sure.	1/31/2018 10:10 PM
32	Reduce amount of lanes on 511a. More well lit streets. More enforcement of cell phone usage while driving. Zero tolerance	1/26/2018 11:11 PM
33	Bike routes would be great and I also think a bike safety course in coordination with the middle school gym classes would also be great.	1/26/2018 7:58 AM
34	Bike racks in commercial district.	1/24/2018 7:29 PM
35	add more street lights so it isn't so dark enforce bicyclists laws by those who ride in groups on weekends taking up entire lanes of road. Add bike lanes to roads and more crosswalks	1/24/2018 1:42 PM
36	I'm a runner so understand that this is actually in reference to running, not cycling. I now live in Rlverdale, but my running routes still take me into PL.	1/24/2018 12:14 PM
37	Cars parked on sidewalks	1/24/2018 9:21 AM
38	Install sidewalks where there are none	1/23/2018 11:54 PM
39	light crosswalks for night walking. better street lighting -always see people walking with flashlights. doggie bag stations along lakeside to keep park and sidewalks clean	1/23/2018 10:53 PM
40	add sidewalk	1/23/2018 9:57 PM
41	Bike lanes and have sidewalks on busy streets	1/23/2018 9:44 PM
42	More sidewalks! We have a lot of walkers and runners in the Ringwood Ave/garden road neighborhood	1/23/2018 9:40 PM
43	Some streets have no sidewalks. They need sidewalks. More bike lanes.	1/23/2018 9:21 PM
44	How about the safety of motorists and people not driving the minimum speed limit, using the turn lane as a merge lane in heavy traffic causing other motorists to slam on brakes, bikes riding in the middle of the road.... Personally, I think this is a waste of our towns time and resources. We should be teaching our children to ride bikes responsibly and not down the middle of Colfax Ave. Growing up in this town as well as now raising my family here, I was taught which streets to walk home using and which to stay off of and am/will be doing the same for my children... why we need a project to ensure ALL streets are bike and pedestrian friendly is beyond me!	1/23/2018 7:47 PM
45	I think there needs to be better lighting for walking and biking at night. Many streets are dark, and I often feel unsafe on those streets.	1/23/2018 7:44 PM
46	Speed bumps on main steeets to slow drivers, increase lighting on streets, clearer pedestrian markings on main streets, bike lanes on main streets (ie: Colfax), put a bike trail on the north side	1/23/2018 7:42 PM
47	Better lighting and signage.	1/23/2018 7:35 PM
48	Bike lanes	1/23/2018 7:00 PM

Pompton Lakes Complete Streets

49	Enforce the speed limit on the roads. Crack down on distracted drivers. Need a bike path.	1/23/2018 6:59 PM
50	None	1/23/2018 6:07 PM
51	Need better lighting. These streets are way too dark and street lights are very outdated. Street signs are not sufficient either. You should not feel unsafe walking around at dusk in the middle of the warmer months!	1/23/2018 5:49 PM
52	Bike lanes, nice sidewalks downtown that don't stain because they were actually sealed properly unlike the current ones	1/23/2018 5:17 PM
53	Yield signs for pedestrians and bicyclists. Closer watch on neighborhood speeders.	1/23/2018 5:10 PM
54	Bike lanes and cameras	11/11/2017 11:28 AM
55	Possibly better traffic control, sidewalks and more visibility of crosswalks on Ringwood Ave between Wanaque and Hamburg. Perhaps some kind of traffic signal for pedestrians at the West Lenox crosswalk on Ringwood Ave. I really have no idea for the others (I'm less familiar with those areas and what the local residents would even want).	8/11/2017 9:05 AM
56	Bike only lanes, more parking downtown and reducing speeding on Dawes highway.	8/7/2017 10:19 PM
57	Bike lane and speeding fines doubled	8/7/2017 9:18 PM
58	Have a bike lane. Enforce homeowners to repair/fix sidewalks, so you can actually walk on the sidewalks and not be forced to walk in the road.	8/7/2017 10:08 AM
59	Sidewalks & bike path	8/7/2017 9:03 AM
60	Speed bump on Jefferson and fix the roads especially on Lakeside	8/7/2017 8:23 AM
61	Better sidewalks and curbs	8/7/2017 12:28 AM
62	Enforce Speed Limits	8/6/2017 4:24 PM
63	Sidewalks and/or bike lane	8/5/2017 10:54 PM
64	Have a nice scenic place to ride bikes	8/5/2017 10:47 PM
65	Blinking signs for motorists to slow down?	8/5/2017 9:16 AM
66	Drivers slowing down and stopping at STOP signs, brighter/better lighting, better sidewalk and road surfaces including road markings (crosswalks & lines).	8/5/2017 7:55 AM
67	Bike lanes. More street lights and traffic lights. More sidewalks and a walking path around the lake.	8/5/2017 4:19 AM
68	Maybe a bicyclist path, better lighting at night, more crosswalk and maybe a little bit more surveillance for of aggressiveness and speeding.	8/4/2017 10:26 PM
69	Enforce the laws on the books. You don't need any more changes/improvements	8/4/2017 5:18 PM
70	Sidewalks, flashing sign to alert for pedestrian crossing And reminder of speed limits. Enforcing speed limits too.	8/4/2017 3:39 PM
71	Hold a special town meeting and ask the residents all there concerns and since we pay the taxes we should voice our opinion and actually have action taken in our favor.	8/4/2017 12:01 PM
72	Noted above.	8/4/2017 9:56 AM
73	Enforcement of laws (speeding, pedestrian right of way in crosswalk, texting and driving, blocked sidewalks), clearly marked pedestrian walkways, repaired sidewalks at some locations, ensuring a clear path i.e. No cars blocking sidewalks and bushes cut back to clear path.	8/4/2017 9:08 AM
74	Build a paved walking/bike path connecting area towns or make biking lines on the streets	8/4/2017 8:37 AM
75	Sidewalks should be added on Lakeside. There are many streets that are not well lit. We tried to get a street light put on our block last summer. I brought JCPL paper to Borough Administrator and said police would do a light assessment. We received no response and no light. I know it's the homeowner's responsibility, but the quality of the sidewalks is poor as well.	8/4/2017 8:35 AM
76	Dedicated bike paths.	8/4/2017 7:05 AM
77	Bike lanes	8/4/2017 6:33 AM
78	Property owners need to "purchase as a group" to get their sidewalks repaired. Perhaps if the town negotiated as a whole, the individual costs would be minimal?	8/4/2017 5:39 AM

Pompton Lakes Complete Streets

79	Creation of bicycle lanes and creation of additional crosswalks.	8/3/2017 10:56 PM
80	By increasing safety awareness for drivers, bicyclist and walkers to share and respect the roads	8/3/2017 10:27 PM
81	make Wanaque ave a one way. widen sidewalks. make "room" for a destination day, night. let business do the rest by using increased walkways for outdoor eating, drinking, activities, etc.	8/3/2017 10:23 PM
82	Drivers follow speed limits, implementation of speed bumps, or CHILDREN PLAYING signs, one side of the street parking, more police surveillance instead of just giving parking tickets.	8/3/2017 10:15 PM
83	Nothing	8/3/2017 10:09 PM
84	make sure vehicles follow speed limit and are policed	8/3/2017 10:09 PM
85	More defined bike paths (especially as a driver) could be effective. While a sidewalk like the one found in Mountain Lakes (separated from the street by a stretch of foliage / grass) is ideal, maybe a defined path on Ramapo Avenue is more cost effective.	8/3/2017 9:58 PM
86	Bike lanes that are slightly wider and a few more speed indicators like the one by the library.	8/3/2017 9:58 PM
87	Bike lanes, more sidewalks	8/3/2017 9:51 PM
88	Put in sidewalks down the entire Ringwood Ave, especially from Hamburg to Wanaque Sidewalks down Ringwood towards Carlough Field would be safer with all the sports traffic, or speed bumps on that road near the field.	8/3/2017 9:46 PM
89	Nothing	8/3/2017 9:24 PM
90	Bike lanes with curbs and reflective lines	8/3/2017 9:13 PM
91	Crosswalk signs, fine sign postings, crosswalk campaign for motorists (like handing out pamphlets at a stoplight), reduced speed limits, more signage reminding people to drive with caution.	8/3/2017 8:40 PM
92	Designated bike lanes on busiest roads.	8/3/2017 8:37 PM
93	Enforce the helmet law that exists.	8/3/2017 8:27 PM
94	Bike lanes	8/3/2017 8:26 PM
95	Speed bumps on Riverview Rd and cut through roads	8/3/2017 8:23 PM
96	Besides sidewalks that are not level, there are a lot of bushes growing too close to sidewalk, forcing 2 people to walk in the street.	8/3/2017 8:21 PM
97	Have JCPL install brighter lights all over the entire town	8/3/2017 8:13 PM
98	Add side walks and make people trim shrubs that are completely blocking the sidewalks	8/3/2017 8:11 PM
99	Slow cars down on Ramapo Ave, wider sidewalks on Wanaque Ave	8/3/2017 8:08 PM
100	Police enforce crosswalk laws	8/2/2017 7:20 PM
101	Driver safety Bike lanes and paths Walking and running paths	8/2/2017 10:40 AM
102	Better street lighting on roads. Especially on Ramapo Ave and Ringwood Ave.	8/1/2017 9:11 PM
103	Sidewalks	8/1/2017 9:10 PM
104	Speed bumps on some streets for speeders	8/1/2017 8:26 PM
105	Cycle parking and moresidewalks	8/1/2017 8:15 PM
106	More bike lanes , more lights	8/1/2017 8:07 PM
107	Traffic aide	8/1/2017 8:03 PM
108	Add more bike lanes on major roads	8/1/2017 7:59 PM
109	Add crosswalks and monitor speed	8/1/2017 7:34 PM
110	Driver and vehicle user education to expect alternate road users	8/1/2017 7:25 PM
111	Yield to pedestrians signs and ticket more!!!	8/1/2017 7:05 PM
112	The entire section of DuPont PI & Howard st with the speeders especially near the new pocket park. WE NEED A 4 WAY STOP SIGN !!!!	8/1/2017 6:33 AM

Pompton Lakes Complete Streets

113	More sidewalks on main streets and repaved sidewalks. Too many pot holes and uneven cements - I've fallen and tripped multiple times this summer due to lack of uneven pavement.	8/1/2017 6:29 AM
114	Education - which side to ride a bicycle on, rules the bicycles should follow, which side to walk on when not on the sidewalk, etc.	7/31/2017 1:02 PM
115	Fix sidewalks, create bike lanes, more enforcement of 25 mph speed limit. Implement plan similar to Bloomingdale a few years ago and fix sidewalks and incorporate over a long period of time into residents taxes	7/31/2017 10:42 AM

APPENDIX G

PUBLIC OUTREACH SURVEY RESULTS

Pompton Lakes - Street Cross Section Options

Place a sticker in the space that you would feel most comfortable Biking ●, Walking ●, and Driving ●.

Bike Lanes	Sharrows	One Bike Lane	Shoulders	No Separation

Key:



Bike Lane: A dedicated space for bicyclists. Cannot be used by other modes of transportation.

Sharrow: A shared-lane marking, indicating to bicyclists where they should be biking within the roadway and alerting drivers of bicyclists.

APPENDIX H

STEERING COMMITTEE MEETING MINUTES

Meeting Summary

Project: Pompton Lakes Complete Streets Plan

Date: February 8, 2018

Subject: First Steering Committee Meeting

Time: 9:30 AM

By: Amy Wong

Place: Borough Council Chambers

Attendees: Representatives from the Borough of Pompton Lakes, Pompton Lakes Board of Education; Pompton Lakes Police Department; Passaic County Department of Planning, TransOptions TMA, and Michael Baker International, Inc. (Michael Baker). Sign in sheet attached.

1. Welcome and Introductions

The first Steering Committee meeting was held for the Pompton Lakes Complete Streets Implementation Plan at 9:30 AM on Thursday February 8, 2018 at the Pompton Lakes Borough Council Chambers. The meeting began with the attendees introducing themselves.

2. Project Background

Steven Wong from Michael Baker provided a project background and introduction to Complete Streets to the attendees. Mr. Wong then proceeded to explain the role of the Project Steering Committee to the attendees. The project scheduled and status to date was then presented.

3. Project Surveys

Project Surveys were previously distributed at National Night Out in 2017, additionally, the survey was made available online. Amy Wong from Michael Baker provided the results of the survey responses for the project. Copies of the surveys were distributed to the attendees for their input.

4. Study Corridors

The attendees were informed of the corridors identified as study locations based on the survey responses and previous input from the Borough of Pompton Lakes.

5. Action Items/Next Steps

- Michael Baker will provide a Spanish translated version of the survey to the Borough for distribution.
- Michael Baker will complete data collection on the identified study locations, analyze all the data and prepare concepts for steering committee input.

Attendees:

William Riviere – New Jersey Department of Transportation

Erik DeLine – Borough of Pompton Lakes

Kevin Boyle – Borough of Pompton Lakes

Paul Amoroso – Pompton Lakes Schools

Ryan Cichon – Pompton Lakes Police Department

Jason Miranda – Passaic County Planning

Justin Jenkins – TransOptions

Steven Wong – Michael Baker

Amy Wong – Michael Baker

Meeting Sign In



Project: Pompton Lakes Pedestrian and Bicycle Improvement Project
Steering Committee Meeting #1

Date: 2/8/2018
Time: 9:30 AM

Place/Room: Pompton Lakes Council Chambers, 25 Lenox Avenue

	PRINT NAME	ORGANIZATION	PHONE	EMAIL
1.	William Riviere	NSPOT	609 530 4646	william.riviere@nj.dot.gov
2.	Justin Jenkins	TransOptions	973-267-7600	jjenkins@transoptions.org
3.	Paul Anicuro	PL BOE	973 835-7100	paul.anicuro@plps.org
4.	Erik DeLine	Pompton Lakes	845 826 1408	edeline@pomptonlakes-nj.gov
5.	Jason Miranda	Passaic County Planning	973 569 4044	jasonm@passaiccountynj.org
6.	Kevin Boyce	Pompton Lakes	973-835-0143 (233)	AD.M.0157@aol.com - Pompton Lakes - NJ 900
7.	Ryan Cichon	PLPD	973-835-0400	rcichon@pomptonlakespolice.org
8.	Amy Wong	Michael Baker	609 807 9582	amywong@michaelbakerintl.com
9.	Steven Wong	Michael Baker	609 807 9556	swong@michaelbakerintl.com
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Meeting Summary

Project: Pompton Lakes Complete Streets Plan

Date: July 12, 2018

Subject: Second Steering Committee Meeting

Time: 9:30 AM

By: Andrew Lappitt

Place: Borough Council Chambers

Attendees: Representatives from the Borough of Pompton Lakes, Pompton Lakes Schools; Pompton Lakes Police; Passaic County Department of Planning, TransOptions TMA, and Michael Baker International, Inc. (Michael Baker); See attached attendees list.

1. Welcome and Introductions

The second Steering Committee meeting was held for the Pompton Lakes Complete Streets Implementation Plan at 9:30 AM on Thursday July 12, 2018 at the Pompton Lakes Borough Council Chambers. The attendees introduced themselves and then Andrew Lappitt of Michael Baker provided a brief background of the project with a PowerPoint presentation.

2. Proposed Roundabout Concepts

Joe Milanese of Michael Baker began discussing the roundabout improvements. Michael Lysicatos of Passaic County mentioned that Passaic County is willing to propose roundabouts on its roads. He also mentioned that the County did a Road Diet on Paterson-Hamburg Turnpike several years ago. Mr. Milanese began by presenting the “peanut” roundabouts at Wanaque Avenue and Passaic Avenue and Paterson-Hamburg Turnpike and Ramapo Avenue. Mayor Michael Serra asked Mr. Milanese if these “peanut” roundabouts have been implemented anywhere and Mr. Milanese mentioned that there are none in New Jersey, but some have been implemented in Massachusetts.

Mr. Milanese also handed out crash diagrams to the Steering Committee that were developed to show crash types and frequency of crashes that occurred at the intersections with proposed roundabouts. Mr. DeLine mentioned that some drivers avoid certain intersections because they feel they are unsafe.

Roundabout at Paterson-Hamburg Turnpike and Ramapo Avenue (Peanut Roundabout)

Kevin Boyle, the Borough Administrator asked why a roundabout is needed at Paterson-Hamburg Turnpike and Ramapo Avenue and if there are any issues with the existing intersection. Mr. Milanese said the roundabout allows traffic flow without stopping and is a safe alternative for skewed intersections. Mr. Lysicatos suggested that the project team “square up” the pedestrian crossing on the north quadrant of the intersection which would bring the crossing closer to the existing crossing, allowing for shorter pedestrian crossings. He also mentioned that this roundabout could be less skewed if possible.

Roundabout at Wanaque Avenue and Passaic (Peanut Roundabout)

The Steering Committee considered this roundabout acceptable and preferred the peanut roundabout over the alternative that consisted of two separated roundabouts. The Steering Committee mentioned

that this alternative would provide pedestrian access from Hamburg Turnpike to Wanaque. Lieutenant Ryan Cichon said this intersection is currently the most dangerous to drive in Pompton Lakes.

Roundabout at Paterson-Hamburg Turnpike and Lincoln Avenue

The Steering Committee considered this roundabout acceptable.

Additional Note: Mr. Lysicatos recommended at Paterson-Hamburg Turnpike and Center Street to eliminate one of the two Center Street entrances on the south quadrant of the intersection. The one remaining entrance should be adjusted to a 90 degree angle.

Roundabout at Paterson-Hamburg Turnpike and Ringwood Avenue

The Steering Committee considered this roundabout acceptable.

3. Proposed Raised Intersection Concepts and Raised Crosswalk Concepts

Mr. Milanese reviewed the proposed raised intersections with the Steering Committee. Mr. Lysicatos mentioned there are no raised intersections in Passaic County but his department is not opposed to them. The County has implemented bump outs with trees and green infrastructure without having an issue with snowplows. Mayor Serra liked the location of the proposed raised intersections near the Pompton Lakes High School because many high school students cross at both locations. He stressed that he is looking for safer ways to get students across Wanaque Avenue. Mr. Milanese also mentioned that the raised intersections could be constructed in phases, where the first phase includes bump outs and, if there are no issues, could be upgraded to raised intersections later on.

Raised Intersection at Lincoln Avenue and Pequannock Avenue

The Steering Committee considered this acceptable.

Raised Intersection at Wanaque Avenue and Babcock

The Steering Committee considered this acceptable.

Raised Intersection at Wanaque Avenue and Lenox Avenue

The Steering Committee considered this acceptable.

Raised Intersection at Willow Avenue and Riverdale Blvd

The Steering Committee considered this acceptable.

Raised Crosswalk at Ramapo Avenue and Lenox

Mr. DeLine mentioned that the school at this intersection already narrows this crossing for pedestrians using cones during the school day, therefore this location is acceptable for a raised crosswalk. Mr. Lysicatos added that Ramapo was the first road in Passaic County with a proposed full bike lane.

Intersection Improvements at Riverdale Blvd and Riverdale Road

Mr. DeLine mentioned that he would like the project team to reassess this intersection and develop new recommendations.

4. Sidewalk Inventory and Potential Local and Regional Connections

Mr. Lysicatos mentioned that the County will work with the Borough on sidewalk implementation, but he also mentioned that sidewalks can be “budget busters.” Mr. Lysicatos recommended a shared use path along the west side of Ringwood Avenue near South Twin Lake which could provide a connection to the Highlands Rail Trail north of Pompton Lakes in Wanaque. The recommended regional connection would guide bicyclists and pedestrians down this shared use path from Wanaque into Pompton Lakes and then guide them east on Wanaque Avenue to Hamburg Turnpike into Wayne. Mr. DeLine mentioned that the plan should provide additional connections to Cannonball Trail from the Highlands Rail Trail and Downtown Pompton Lakes. Michael Baker proposed Pipeline Path to connect to the intersection of Riverdale Road and Riverdale Blvd with a bridge over the Wanaque. The Steering Committee wants to keep this in the plan but understands that a bridge would cost between \$50,000 and \$250,000 depending on the type of bridge. Mr. Lysicatos also recommended a pedestrian bridge connecting Pompton Lakes to Riverdale over the Pequannock River.

5. Proposed Lakeside Avenue and Colfax Avenue One-Way Street Concept

The Steering Committee did not find the one-way pair streets along Lakeside and Colfax acceptable.

6. Conceptual Cross Sections

The Steering Committee did not find four-foot bike lanes to be acceptable for the cross sections.

Riverdale Blvd

- The Steering Committee liked the parking on one side of the road. Additionally, this Alternative would provide a good connection to a bike path (NY-NJ Bike Path?).
- The logical selling point of full bike lanes on this road is that they can provide a connection to the Willow Avenue Fields.
- If Sharrows are implemented rather than bike lanes, the “green back” sharrows should be used.
- The Steering Committee recommended that Alternative 1 have a Sharrow on one side and a bike lane on the other side.
- The Steering Committee recommended that Alternative 2 have 5’ bike lanes, 10’ travel lanes and 7’ parking lanes.

Ringwood Avenue North

- In the 30’ cross sections, the Steering Committee recommended 11’ lanes with 4’ shoulders.

Ringwood Avenue South

- There is no issue with construction on the west side of this road because a cemetery borders the road.

Dawes Highway

- The Steering Committee recommended that the north side of Dawes Highway would be good for “green back” Sharrows or bike lanes because there are not many driveways or fronts of homes.
- For Alternative 3, the Steering Committee recommended a Sharrow on one side and a bike lane on the other side.

7. Action Items/Next Steps

- Roundabout at Paterson-Hamburg Turnpike and Ramapo Avenue: “Square up” the north quadrant of this proposed roundabout to create a shorter and safer pedestrian crossing. Additionally, adjust this intersection to have less of a “skew,” if possible.
- Recommend a concept where one of the south quadrant entrances to Center Street from Paterson-Hamburg Turnpike is eliminated and replaced with one 90-degree angle entrance.
- Add trees and green infrastructure to the bump outs of the raised intersections.
- Reassess intersection of Riverdale Blvd and Riverdale Road for improvement recommendations.
- Remove 4’ bike lanes from cross sections and aerials.
- Label a potential bridge connection over the Wanaque River from Pipeline Path to Riverdale Blvd.
- Create additional connections to Cannonball Trail from Downtown and Ringwood Avenue.
- Identify a pedestrian/bicycle route beginning on Ringwood south of I-287, continuing east on Wanaque Avenue, then south on Hamburg Turnpike to Wayne.
- Revise comments on cross section concepts listed in the summary above.

Attendees:

Ryan Cichon – Pompton Lakes Police

Erik DeLine – Borough of Pompton Lakes

Michael Serra – Borough of Pompton Lakes

Kevin Boyle – Borough of Pompton Lakes

Paul Amoroso – Pompton Lakes Schools

Michael Lysicatos – Passaic County Planning

Liz Ward – Passaic County Planning

Daniel Callas – TransOptions, Inc

Joe Milanese – Michael Baker

Andrew Lappitt – Michael Baker

Meeting Sign In

Project: Pompton Lakes Complete Streets Plan Steering Committee Meeting

Date: 7/12/2018

Place/Room: Borough Council Chambers

Time: 9:30 AM

PRINT NAME	ORGANIZATION	PHONE	EMAIL
1. Ryan Cichon	PLPID	973-835-0100	vcichon@pomptonlakespolice.org
2. Erik Deline	Pompton Lakes	845-826-1408	edeline@pomptonlakes-nj.gov
3. Paul Amoroso	Pompton Lakes Schools	973-835-7100	paul.amoroso@plps.org
4. Kevin F. Boyle	Pompton Lakes	973-835-0443	Admin@pomptonlakes-nj.gov
5. Michael Lysicatos	Passaic County	973-569-4047	mlyscat@passaiccounty.nj.org
6. Liz Ward	Passaic County	973-569-4045	lizward@passaiccounty.nj.org
7. DAN CALLAS	TRANS OPTIONS	973-267-7600	dcallas@transoptions.org
8. Andrew Lyggett	Michael Baker Intl	609-807-9610	andrew.lyggett@michaelbakernj.com
9. Joe Mihrose	Michael Baker	609-807-1542	jmhrose@michaelbakernj.com
10. Michael Serra	Mayor of Pompton Lakes	973-835-0443 x240	m.serra@pomptonlakes-nj.gov
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APPENDIX I

TRAFFIC SIGNAL TIMINGS

Project: Pompton Lakes LTA	
Major Road Name: Hamburg Turnpike	
Minor Road Name(s): Ramapo Ave.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Vehicular Interval Calculations			Notes
Phase A - Hamburg Turnpike ROW			
Intersection Width (P) =	138 ft		
Approach Speed (V) =	35 mph or 51.345 ft/s		
Approach Grade (G) =	0.000 %		
Length of Vehicle (L) =	20 ft		
Perception-Reaction Time (t) =	1 s		
Deceleration Rate (a) =	10 ft/s ²		
Method 1, based on ITE's Manual on Traffic Signal Design:			
Yellow Change Interval (Y) =	$t + V / (2a + 64.4G) =$	3.567 s	
Red Clearance Interval (R) =	$(W + L) / V =$	3.077 s	
Total Clearance Interval (VCI) =	Y + R =	6.644 s	
Method 2, based on NJDOT's Roadway Design Manual:			
Yellow Change Interval (Y) =	$V \text{ (in mph)} / 10 =$	4.000 s	
Total Clearance Interval (VCI) =	$t + V/2a + (W + L)/V =$	6.644 s	
Red Clearance Interval (R) =	VCI - Y =	2.644 s	
Existing Intervals:	Y = 4	R = 3	Existing intervals are per NJDOT policy
Proposed Intervals:	Y = 4	R = 3	

Phase B - Ramapo Ave. ROW			Notes
Intersection Width (P) =	105 ft		
Approach Speed (V) =	25 mph or 36.675 ft/s		
Approach Grade (G) =	0.000 %		
Length of Vehicle (L) =	20 ft		
Perception-Reaction Time (t) =	1 s		
Deceleration Rate (a) =	10 ft/s ²		
Method 1, based on ITE's Manual on Traffic Signal Design:			
Yellow Change Interval (Y) =	$t + V / (2a + 64.4G) =$	2.834 s	
Red Clearance Interval (R) =	$(W + L) / V =$	3.408 s	
Total Clearance Interval (VCI) =	Y + R =	6.242 s	
Method 2, based on NJDOT's Roadway Design Manual:			
Yellow Change Interval (Y) =	$V \text{ (in mph)} / 10 =$	3.000 s	
Total Clearance Interval (VCI) =	$t + V/2a + (W + L)/V =$	6.242 s	
Red Clearance Interval (R) =	VCI - Y =	3.242 s	
Existing Intervals:	Y = 3	R = 3	Red Clearance Interval should be increased to 4s.
Proposed Intervals:	Y = 3	R = 4	

Project: Pompton Lakes LTA	
Major Road Name: Hamburg Turnpike	
Minor Road Name(s): Ramapo Ave.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Pedestrian Interval Calculations		Notes
Phase A - Crossing Ramapo Ave.		
<i>Condition 1, based on 2009 MUTCD, Section 4E.06, Paragraph 7:</i>		No push button to cross minor roadway.
CW Crossing Distance = 52 ft		
Walking Speed = 3.5 ft/s		
Pedestrian Clearance Time = Distance / Speed = 14.857 s		
<i>Condition 2, based on 2009 MUTCD, Section 4E.06, Paragraph 14:</i>		Existing intervals are per NJDOT policy
PB-CW Distance = 6 ft		
Total Crossing Distance = 58 ft		
Walking Speed = 3 ft/s		
Total Crossing Time = Distance / Speed = 19.333 s		
<i>Existing Intervals:</i>	Pedestrian Walk = 20	
	Pedestrian Change (FDW) = 23	
	Buffer = 3	
<i>Proposed Intervals:</i>	Pedestrian Walk = 20	
	Pedestrian Change (FDW) = 23	
	Buffer = 3	

<p>Project: Pompton Lakes LTA Major Road Name: Hamburg Turnpike Minor Road Name(s): Ramapo Ave. Prepared By: JAM Date: May 22, 2018 Checked By: Date:</p>
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Phase B - Crossing Hamburg Turnpike		Notes
<p>Condition 1, based on 2009 MUTCD, Section 4E.06, Paragraph 7: CW Crossing Distance = 50 ft Walking Speed = 3.5 ft/s Pedestrian Clearance Time = Distance / Speed = 14.286 s</p>		<p><i>Existing intervals are per NJDOT policy; however, red clearance interval should be increased to 4s. Per vehicle clearance calc.</i></p>
<p>Condition 2, based on 2009 MUTCD, Section 4E.06, Paragraph 14: PB-CW Distance = 8 ft Total Crossing Distance = 58 ft Walking Speed = 3 ft/s Total Crossing Time = Distance / Speed = 19.333 s</p>		
Existing Intervals:	Pedestrian Walk = 5	
	Pedestrian Change (FDW) = 16	
	Buffer = 3	
Proposed Intervals:	Pedestrian Walk = 5	
	Pedestrian Change (FDW) = 16	
	Buffer = 4	

Project: Pompton Lakes LTA	
Major Road Name: Paterson-Hamburg Turnpike	
Minor Road Name(s): Ringwood Ave.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Vehicular Interval Calculations			Notes
Phase A - Paterson-Hamburg Turnpike EB Lead			<i>Protected-permissive LT operation</i>
Intersection Width (P) =	ft		
Approach Speed (V) =	35 mph or 51.345 ft/s		
Approach Grade (G) =	0.000 %		
Length of Vehicle (L) =	20 ft		
Perception-Reaction Time (t) =	1 s		
Deceleration Rate (a) =	10 ft/s ²		
<i>Method 1, based on ITE's Manual on Traffic Signal Design:</i>			
Yellow Change Interval (Y) =	$t + V / (2a + 64.4G) =$	3.567 s	
Red Clearance Interval (R) =	$(W + L) / V =$	0.390 s	
Total Clearance Interval (VCI) =	Y + R =	3.957 s	
<i>Method 2, based on NJDOT's Roadway Design Manual:</i>			
Yellow Change Interval (Y) =	$V \text{ (in mph)} / 10 =$	4.000 s	
Total Clearance Interval (VCI) =	$t + V/2a + (W + L)/V =$	3.957 s	
Red Clearance Interval (R) =	VCI - Y =	-0.043 s	
Existing Intervals:	Y = 3	R = 0	<i>Existing intervals are per NJDOT policy</i>
Proposed Intervals:	Y = 3	R = 0	

Phase B - Paterson-Hamburg Turnpike ROW			Notes
Intersection Width (P) = 110 ft			<i>EB left lane stop line pushed away from intersection due to lead left-turn</i>
Approach Speed (V) = 35 mph or 51.345 ft/s			
Approach Grade (G) = 0.000 %			
Length of Vehicle (L) = 20 ft			
Perception-Reaction Time (t) = 1 s			
Deceleration Rate (a) = 10 ft/s ²			
<i>Method 1, based on ITE's Manual on Traffic Signal Design:</i>			
Yellow Change Interval (Y) =	$t + V / (2a + 64.4G) =$	3.567 s	
Red Clearance Interval (R) =	$(W + L) / V =$	2.532 s	
Total Clearance Interval (VCI) =	Y + R =	6.099 s	
<i>Method 2, based on NJDOT's Roadway Design Manual:</i>			
Yellow Change Interval (Y) =	$V \text{ (in mph)} / 10 =$	4.000 s	
Total Clearance Interval (VCI) =	$t + V/2a + (W + L)/V =$	6.099 s	
Red Clearance Interval (R) =	VCI - Y =	2.099 s	
Existing Intervals:	Y = 4	R = 3	<i>Existing intervals are per NJDOT policy</i>
Proposed Intervals:	Y = 4	R = 3	

Project: Pompton Lakes LTA	
Major Road Name: Paterson-Hamburg Turnpike	
Minor Road Name(s): Ringwood Ave.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Phase C - Ringwood Ave. ROW		Notes
Intersection Width (P) =	120 ft	
Approach Speed (V) =	35 mph or 51.345 ft/s	
Approach Grade (G) =	0.000 %	
Length of Vehicle (L) =	20 ft	
Perception-Reaction Time (t) =	1 s	
Deceleration Rate (a) =	10 ft/s ²	
Method 1, based on ITE's Manual on Traffic Signal Design:		
Yellow Change Interval (Y) = $t + V / (2a + 64.4G) =$	3.567 s	
Red Clearance Interval (R) = $(W + L) / V =$	2.727 s	
Total Clearance Interval (VCI) = $Y + R =$	6.294 s	
Method 2, based on NJDOT's Roadway Design Manual:		
Yellow Change Interval (Y) = $V \text{ (in mph)} / 10 =$	4.000 s	
Total Clearance Interval (VCI) = $t + V/2a + (W + L)/V =$	6.294 s	
Red Clearance Interval (R) = $VCI - Y =$	2.294 s	
Existing Intervals:	Y = 4 R = 3	Existing intervals are per NJDOT policy
Proposed Intervals:	Y = 4 R = 3	

Pedestrian Interval Calculations			
Phase B - Crossing Ringwood Ave.		Notes	
Condition 1, based on 2009 MUTCD, Section 4E.06, Paragraph 7:			
CW Crossing Distance =	48 ft	No push button to cross minor roadway.	
Walking Speed =	3.5 ft/s		
Pedestrian Clearance Time = Distance / Speed =	13.714 s		
Condition 2, based on 2009 MUTCD, Section 4E.06, Paragraph 14:			
PB-CW Distance =	6 ft		
Total Crossing Distance =	54 ft		
Walking Speed =	3 ft/s		
Total Crossing Time = Distance / Speed =	18.000 s		
Existing Intervals:	Pedestrian Walk =		6
	Pedestrian Change (FDW) =		14
	Buffer =	3	
Proposed Intervals:	Pedestrian Walk =	6	
	Pedestrian Change (FDW) =	14	
	Buffer =	3	
		Existing intervals are per NJDOT policy	

Project: Pompton Lakes LTA	
Major Road Name: Paterson-Hamburg Turnpike	
Minor Road Name(s): Ringwood Ave.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Phase C - Crossing Paterson-Hamburg Turnpike		Notes
Condition 1, based on 2009 MUTCD, Section 4E.06, Paragraph 7: CW Crossing Distance = 50 ft Walking Speed = 3.5 ft/s Pedestrian Clearance Time = Distance / Speed = 14.286 s		
Condition 2, based on 2009 MUTCD, Section 4E.06, Paragraph 14: PB-CW Distance = 8 ft Total Crossing Distance = 58 ft Walking Speed = 3 ft/s Total Crossing Time = Distance / Speed = 19.333 s		
Existing Intervals:	Pedestrian Walk = 10	
	Pedestrian Change (FDW) = 13	
	Buffer = 3	
Proposed Intervals:	Pedestrian Walk = 10	
	Pedestrian Change (FDW) = 15	
	Buffer = 3	

FDW time should be increased to 15s.

Project: Pompton Lakes LTA	
Major Road Name: Ringwood Ave.	
Minor Road Name(s): Wanaque Ave.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Vehicular Interval Calculations			Notes
Phase A - Ringwood Ave. SB Lead			<i>Protected-permissive LT operation</i>
Intersection Width (P) =	ft		
Approach Speed (V) =	35 mph or 51.345 ft/s		
Approach Grade (G) =	0.000 %		
Length of Vehicle (L) =	20 ft		
Perception-Reaction Time (t) =	1 s		
Deceleration Rate (a) =	10 ft/s ²		
Method 1, based on ITE's Manual on Traffic Signal Design:			
Yellow Change Interval (Y) = $t + V / (2a + 64.4G) =$		3.567 s	
Red Clearance Interval (R) = $(W + L) / V =$		0.390 s	
Total Clearance Interval (VCI) = $Y + R =$		3.957 s	
Method 2, based on NJDOT's Roadway Design Manual:			
Yellow Change Interval (Y) = $V \text{ (in mph)} / 10 =$		4.000 s	
Total Clearance Interval (VCI) = $t + V/2a + (W + L)/V =$		3.957 s	
Red Clearance Interval (R) = $VCI - Y =$		-0.043 s	
Existing Intervals:	Y = 3	R = 0	<i>Yellow Change Interval should be increased to 4s.</i>
Proposed Intervals:	Y = 4	R = 0	

Phase B - Ringwood Ave. ROW			Notes
Intersection Width (P) =	95 ft		
Approach Speed (V) =	35 mph or 51.345 ft/s		
Approach Grade (G) =	0.000 %		
Length of Vehicle (L) =	20 ft		
Perception-Reaction Time (t) =	1 s		
Deceleration Rate (a) =	10 ft/s ²		
Method 1, based on ITE's Manual on Traffic Signal Design:			
Yellow Change Interval (Y) = $t + V / (2a + 64.4G) =$		3.567 s	
Red Clearance Interval (R) = $(W + L) / V =$		2.240 s	
Total Clearance Interval (VCI) = $Y + R =$		5.807 s	
Method 2, based on NJDOT's Roadway Design Manual:			
Yellow Change Interval (Y) = $V \text{ (in mph)} / 10 =$		4.000 s	
Total Clearance Interval (VCI) = $t + V/2a + (W + L)/V =$		5.807 s	
Red Clearance Interval (R) = $VCI - Y =$		1.807 s	
Existing Intervals:	Y = 4	R = 2	<i>Existing intervals are per NJDOT policy</i>
Proposed Intervals:	Y = 4	R = 2	

<p>Project: Pompton Lakes LTA Major Road Name: Ringwood Ave. Minor Road Name(s): Wanaque Ave. Prepared By: JAM Date: May 22, 2018 Checked By: Date:</p>			
Phase C - Wanaque Ave. NB Lead			<i>Notes</i>
Intersection Width (P) = ft Approach Speed (V) = 25 mph or 36.675 ft/s Approach Grade (G) = 0.000 % Length of Vehicle (L) = 20 ft Perception-Reaction Time (t) = 1 s Deceleration Rate (a) = 10 ft/s ²			<i>Protected-permissive LT operation</i>
<i>Method 1, based on ITE's Manual on Traffic Signal Design:</i>			
Yellow Change Interval (Y) = $t + V / (2a + 64.4G) =$		2.834 s	
Red Clearance Interval (R) = $(W + L) / V =$		0.545 s	
Total Clearance Interval (VCI) = $Y + R =$		3.379 s	
<i>Method 2, based on NJDOT's Roadway Design Manual:</i>			
Yellow Change Interval (Y) = $V \text{ (in mph)} / 10 =$		3.000 s	
Total Clearance Interval (VCI) = $t + V/2a + (W + L)/V =$		3.379 s	
Red Clearance Interval (R) = $VCI - Y =$		0.379 s	
<i>Existing Intervals:</i>	Y = 3	R = 0	
<i>Proposed Intervals:</i>	Y = 3	R = 0	

Project: Pompton Lakes LTA	
Major Road Name: Ringwood Ave.	
Minor Road Name(s): Wanaque Ave.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Phase D - Wanaque Ave. ROW		Notes
Intersection Width (P) =	100 ft	
Approach Speed (V) =	25 mph or 36.675 ft/s	
Approach Grade (G) =	0.000 %	
Length of Vehicle (L) =	20 ft	
Perception-Reaction Time (t) =	1 s	
Deceleration Rate (a) =	10 ft/s ²	
Method 1, based on ITE's Manual on Traffic Signal Design:		
Yellow Change Interval (Y) = $t + V / (2a + 64.4G) =$	2.834 s	
Red Clearance Interval (R) = $(W + L) / V =$	3.272 s	
Total Clearance Interval (VCI) = $Y + R =$	6.106 s	
Method 2, based on NJDOT's Roadway Design Manual:		
Yellow Change Interval (Y) = $V \text{ (in mph)} / 10 =$	3.000 s	
Total Clearance Interval (VCI) = $t + V/2a + (W + L)/V =$	6.106 s	
Red Clearance Interval (R) = $VCI - Y =$	3.106 s	
Existing Intervals:	Y = 3 R = 3	<i>Red Clearance Interval should be increased to 4s.</i>
Proposed Intervals:	Y = 3 R = 4	

Pedestrian Interval Calculations			
Phase B - Crossing Wanaque Ave.		Notes	
Condition 1, based on 2009 MUTCD, Section 4E.06, Paragraph 7:			
CW Crossing Distance =	60 ft		
Walking Speed =	3.5 ft/s		
Pedestrian Clearance Time = Distance / Speed =	17.143 s		
Condition 2, based on 2009 MUTCD, Section 4E.06, Paragraph 14:			
PB-CW Distance =	6 ft		
Total Crossing Distance =	66 ft		
Walking Speed =	3 ft/s		
Total Crossing Time = Distance / Speed =	22.000 s		
Existing Intervals:	Pedestrian Walk =		7
	Pedestrian Change (FDW) =		18
	Buffer =	3	
Proposed Intervals:	Pedestrian Walk =	7	
	Pedestrian Change (FDW) =	18	
	Buffer =	3	

No push button to cross minor roadway.

Existing intervals are per NJDOT policy

Project: Pompton Lakes LTA	
Major Road Name: Ringwood Ave.	
Minor Road Name(s): Wanaque Ave.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Phase D - Crossing Ringwood Ave.		Notes
Condition 1, based on 2009 MUTCD, Section 4E.06, Paragraph 7: CW Crossing Distance = 58 ft Walking Speed = 3.5 ft/s Pedestrian Clearance Time = Distance / Speed = 16.571 s		
Condition 2, based on 2009 MUTCD, Section 4E.06, Paragraph 14: PB-CW Distance = 8 ft Total Crossing Distance = 66 ft Walking Speed = 3 ft/s Total Crossing Time = Distance / Speed = 22.000 s		
Existing Intervals:	Pedestrian Walk = 4	
	Pedestrian Change (FDW) = 18	
	Buffer = 3	
Proposed Intervals:	Pedestrian Walk = 4	
	Pedestrian Change (FDW) = 18	
	Buffer = 3	

Existing intervals are per NJDOT policy

Project: Pompton Lakes LTA	
Major Road Name: Wanaque Ave.	
Minor Road Name(s): Grove St. / Shopping Ctr.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Vehicular Interval Calculations			Notes
Phase A - Wanaque Ave. NB Lead			<i>Protected-permissive LT operation</i>
Intersection Width (P) =	ft		
Approach Speed (V) =	25 mph or 36.675 ft/s		
Approach Grade (G) =	0.000 %		
Length of Vehicle (L) =	20 ft		
Perception-Reaction Time (t) =	1 s		
Deceleration Rate (a) =	10 ft/s ²		
<i>Method 1, based on ITE's Manual on Traffic Signal Design:</i>			
Yellow Change Interval (Y) =	$t + V / (2a + 64.4G) =$	2.834 s	
Red Clearance Interval (R) =	$(W + L) / V =$	0.545 s	
Total Clearance Interval (VCI) =	$Y + R =$	3.379 s	
<i>Method 2, based on NJDOT's Roadway Design Manual:</i>			
Yellow Change Interval (Y) =	$V \text{ (in mph)} / 10 =$	3.000 s	
Total Clearance Interval (VCI) =	$t + V/2a + (W + L)/V =$	3.379 s	
Red Clearance Interval (R) =	$VCI - Y =$	0.379 s	
Existing Intervals:	Y = 3	R = 0	<i>Existing intervals are per NJDOT policy</i>
Proposed Intervals:	Y = 3	R = 0	

Phase B - Wanaque Ave ROW			Notes
Intersection Width (P) =	120 ft		
Approach Speed (V) =	25 mph or 36.675 ft/s		
Approach Grade (G) =	0.000 %		
Length of Vehicle (L) =	20 ft		
Perception-Reaction Time (t) =	1 s		
Deceleration Rate (a) =	10 ft/s ²		
<i>Method 1, based on ITE's Manual on Traffic Signal Design:</i>			
Yellow Change Interval (Y) =	$t + V / (2a + 64.4G) =$	2.834 s	
Red Clearance Interval (R) =	$(W + L) / V =$	3.817 s	
Total Clearance Interval (VCI) =	$Y + R =$	6.651 s	
<i>Method 2, based on NJDOT's Roadway Design Manual:</i>			
Yellow Change Interval (Y) =	$V \text{ (in mph)} / 10 =$	3.000 s	
Total Clearance Interval (VCI) =	$t + V/2a + (W + L)/V =$	6.651 s	
Red Clearance Interval (R) =	$VCI - Y =$	3.651 s	
Existing Intervals:	Y = 3	R = 4	<i>Existing intervals are per NJDOT policy</i>
Proposed Intervals:	Y = 3	R = 4	

Project: Pompton Lakes LTA	
Major Road Name: Wanaque Ave.	
Minor Road Name(s): Grove St. / Shopping Ctr.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Phase C - Grove St./Shopping Ctr. ROW		Notes
Intersection Width (P) =	85 ft	
Approach Speed (V) =	25 mph or 36.675 ft/s	
Approach Grade (G) =	0.000 %	
Length of Vehicle (L) =	20 ft	
Perception-Reaction Time (t) =	1 s	
Deceleration Rate (a) =	10 ft/s ²	
Method 1, based on ITE's Manual on Traffic Signal Design:		
Yellow Change Interval (Y) = $t + V / (2a + 64.4G) =$	2.834 s	
Red Clearance Interval (R) = $(W + L) / V =$	2.863 s	
Total Clearance Interval (VCI) = $Y + R =$	5.697 s	
Method 2, based on NJDOT's Roadway Design Manual:		
Yellow Change Interval (Y) = $V \text{ (in mph)} / 10 =$	3.000 s	
Total Clearance Interval (VCI) = $t + V/2a + (W + L)/V =$	5.697 s	
Red Clearance Interval (R) = $VCI - Y =$	2.697 s	
Existing Intervals:	Y = 3 R = 3	Existing intervals are per NJDOT policy
Proposed Intervals:	Y = 3 R = 3	

Pedestrian Interval Calculations			
Phase B - Crossing Grove St./Shopping Ctr.		Notes	
Condition 1, based on 2009 MUTCD, Section 4E.06, Paragraph 7:			
CW Crossing Distance =	72 ft	No push button to cross minor roadway.	
Walking Speed =	3.5 ft/s		
Pedestrian Clearance Time = Distance / Speed =	20.571 s		
Condition 2, based on 2009 MUTCD, Section 4E.06, Paragraph 14:			
PB-CW Distance =	6 ft		
Total Crossing Distance =	78 ft		
Walking Speed =	3 ft/s		
Total Crossing Time = Distance / Speed =	26.000 s		
Existing Intervals:	Pedestrian Walk =		12
	Pedestrian Change (FDW) =		21
	Buffer =	3	
Proposed Intervals:	Pedestrian Walk =	12	
	Pedestrian Change (FDW) =	21	
	Buffer =	3	
		Existing intervals are per NJDOT policy	

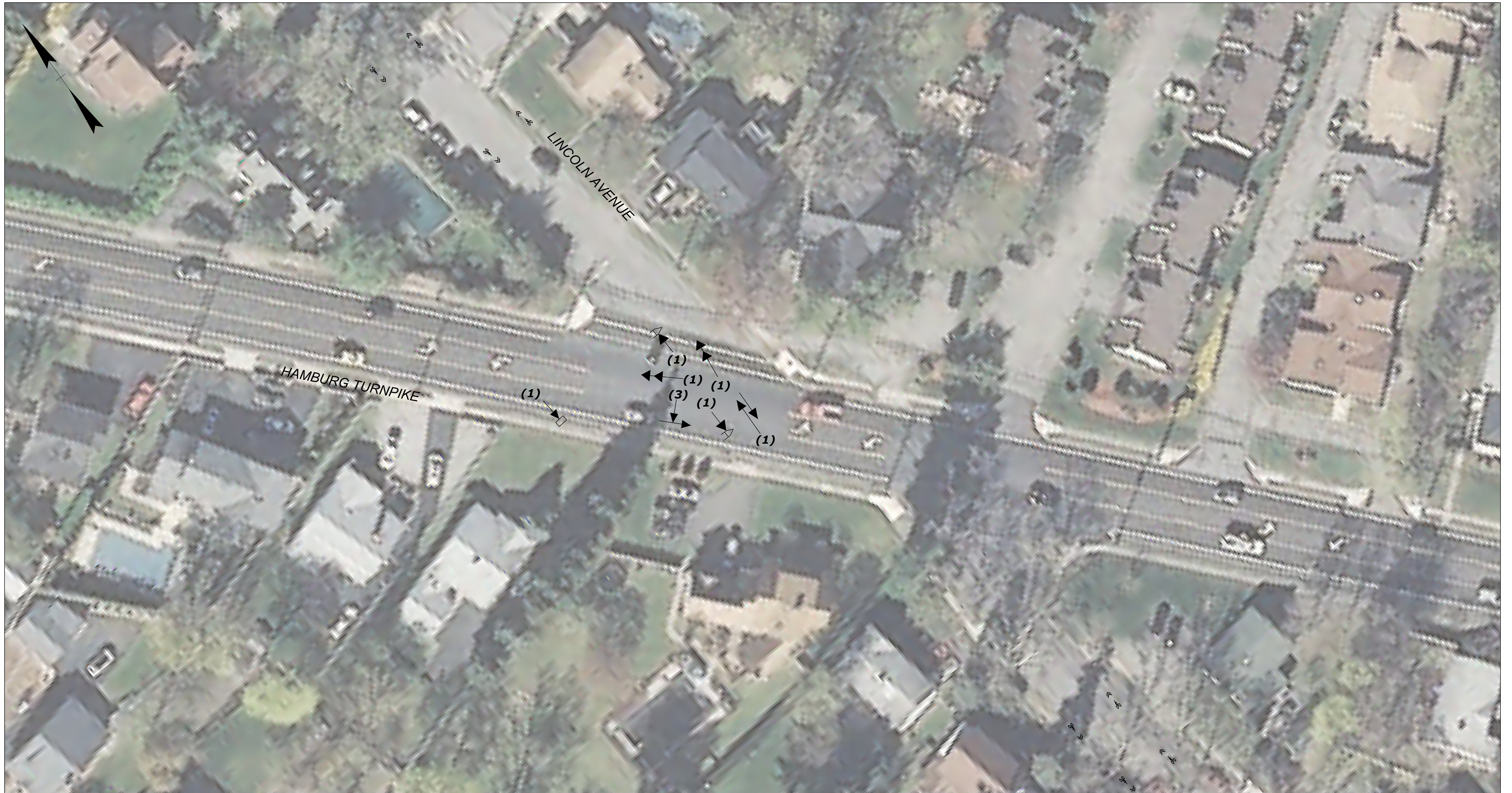
Project: Pompton Lakes LTA	
Major Road Name: Wanaque Ave.	
Minor Road Name(s): Grove St. / Shopping Ctr.	
Prepared By: JAM	Date: May 22, 2018
Checked By:	Date:

Phase C - Crossing Wanaque Ave.		Notes
Condition 1, based on 2009 MUTCD, Section 4E.06, Paragraph 7: CW Crossing Distance = 65 ft Walking Speed = 3.5 ft/s Pedestrian Clearance Time = Distance / Speed = 18.571 s		
Condition 2, based on 2009 MUTCD, Section 4E.06, Paragraph 14: PB-CW Distance = 6 ft Total Crossing Distance = 71 ft Walking Speed = 3 ft/s Total Crossing Time = Distance / Speed = 23.667 s		
Existing Intervals:	Pedestrian Walk = 5	
	Pedestrian Change (FDW) = 19	
	Buffer = 3	
Proposed Intervals:	Pedestrian Walk = 5	
	Pedestrian Change (FDW) = 19	
	Buffer = 3	

Existing intervals are per NJDOT policy

APPENDIX J

CRASHES AT INTERSECTIONS WITH PROPOSED ROUNDABOUTS



LEGEND

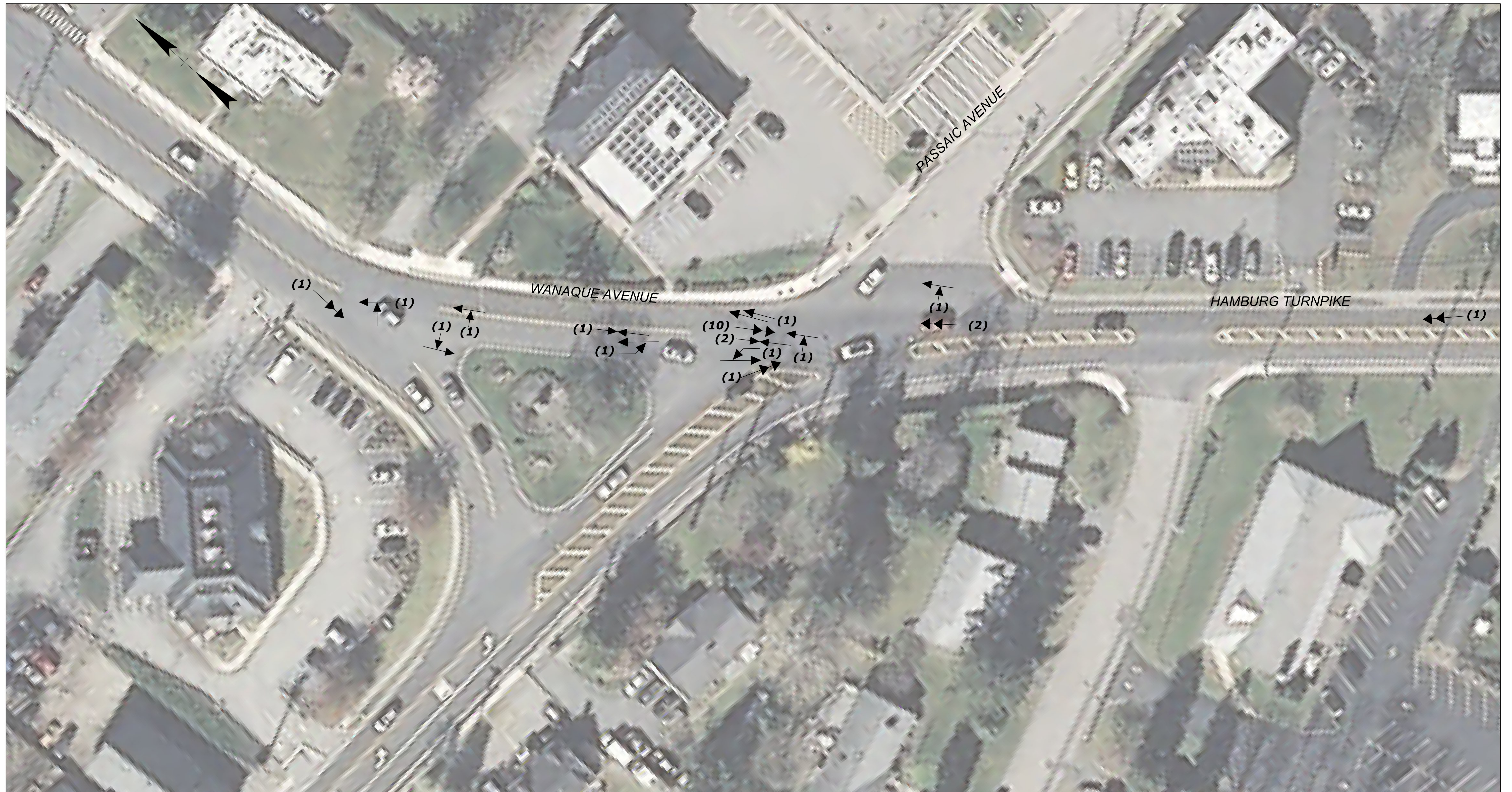
- | | | | | | |
|----|--------------------------------------|----|--------------|----|-----------------------|
| ←← | SAME DIRECTION-REAR END | ◻← | ENCROACHMENT | ◁← | ANIMAL |
| ←← | SAME DIRECTION-SIDE SWIPE | ◻← | PEDALCYCLIST | ⊗← | NON-FIXED OBJECT |
| ↗↗ | LEFT TURN/U-TURN | ◻← | FIXED OBJECT | P← | PEDESTRIAN |
| ←→ | OPPOSITE DIRECTION - SIDE SWIPE | →→ | BACKING | ◻← | STRUCK PARKED VEHICLE |
| ↔↔ | OPPOSITE DIRECTION - HEAD ON/ANGULAR | ⊥ | RIGHT ANGLE | ○← | OTHER |



(X) NUMBER OF CRASHES

C-3
C-4

Crash Locations (2014-2016)	
Borough of Pompton Lakes Pedestrian and Bicycle Improvement Project	
Location	Hamburg Turnpike and Lincoln Avenue
	July 2018





LEGEND

- | | | | | | |
|------|--------------------------------------|----|--------------|----|-----------------------|
| ←← | SAME DIRECTION-REAR END | ⊞← | ENCROACHMENT | ⚠← | ANIMAL |
| ←→ | SAME DIRECTION-SIDE SWIPE | ⊞← | PEDALCYCLIST | ⊞← | NON-FIXED OBJECT |
| ↶↷ | LEFT TURN/U-TURN | ⊞← | FIXED OBJECT | P← | PEDESTRIAN |
| ↷↶ | OPPOSITE DIRECTION - SIDE SWIPE | ↶↷ | BACKING | ⊞← | STRUCK PARKED VEHICLE |
| ↶↷↶↷ | OPPOSITE DIRECTION - HEAD ON/ANGULAR | ↶↷ | RIGHT ANGLE | ○← | OTHER |



(X) NUMBER OF CRASHES

Crash Locations (2014-2016) Borough of Pompton Lakes Pedestrian and Bicycle Improvement Project	
Location	Wanaque Avenue, Passaic Avenue, and Hamburg Turnpike
 July 2018  	

C-1
C-4